



GOSPORT RAILWAY STATION

Spring Garden Lane, Gosport



DEVELOPMENT BRIEF

November 2005

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1.0 BACKGROUND

1.1 Location

1.1.1 Hampshire County Council is owner of the Grade II* listed Gosport Station. This 0.7 hectare site is the former terminus of the London and South Western Railway. It is located about 0.5 km to the north west of Gosport Town Centre between Spring Gardens Lane and George Street, just to the south of the A32 (see figures 1 and 2).

1.2 Objective

1.2.1 The site has not been used for a number of years and over this period the condition of the buildings has deteriorated. The objective is to find a new use that adapts the buildings and secures their long term future. The purpose of this document is to provide both the vision and detailed planning guidance for the adaptation and redevelopment of Gosport Station. It will be

used by potential developers to guide their proposals for the site and by the County Council as a basis for evaluating the design quality of developers' submissions.

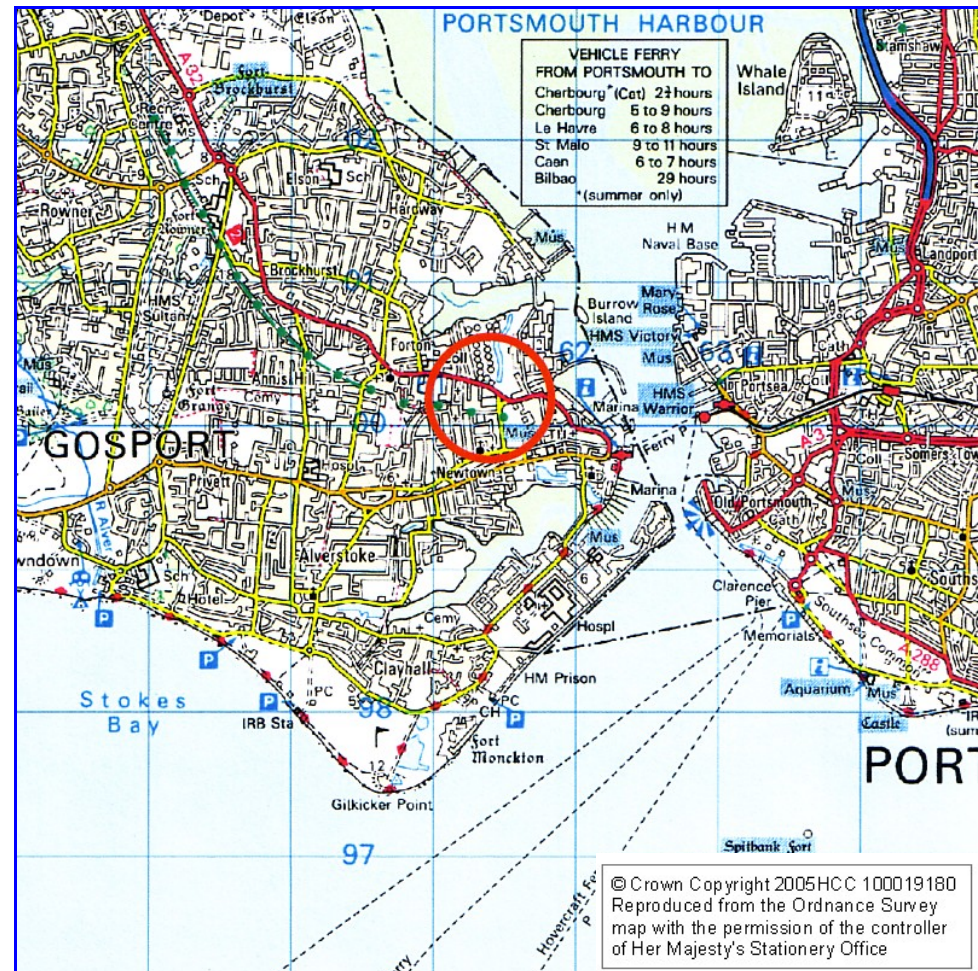


Figure 1

1.3 Consultation

1.3.1 This Development Brief has been produced by Hampshire County Council's Estates officers in conjunction with the Environment Department, Gosport Borough Council and English Heritage. Consultation has been undertaken with local residents by distribution of a leaflet and a meeting with the Gosport Society. The Brief was approved by Hampshire County Council Executive Member (Policy and Resources) on 11th November 2004 and by Gosport Borough Council Transportation and Planning Sub Board on 30th November 2004.

1.4 Method of sale

1.4.1 The site will be advertised for sale on the open market and sold by way of a design offer tender process. This means that the design quality of developer's submissions will be assessed alongside financial offers. Given the sensitive nature of this site, high quality design is essential and proposals that do not adhere to the requirements of this brief are unlikely to be considered.

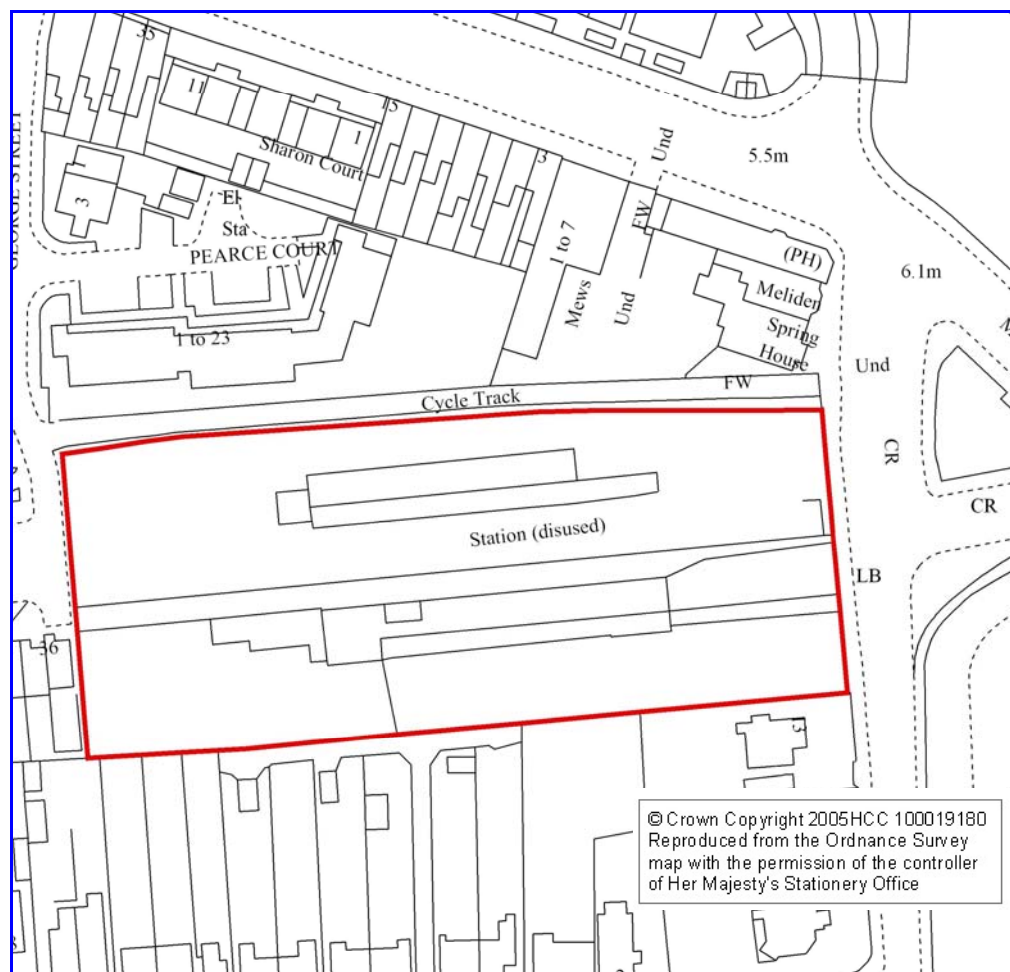


Figure 2

2.0 VISION

2.1 The County Council seeks the adaptation of these buildings with a sympathetic and viable long term use. It envisages a mixed use development potentially including offices, studios, workshops, live work units, restaurant, residential and community uses in the restored station buildings and complementary new development. The detailed design will ensure that the former booking hall, station master's house and colonnade are restored to their original state using traditional materials, that the platforms are retained and the former track bed is clearly recognisable as such. New development is appropriate but this must be complementary and subservient through the use of traditional materials, design details and appropriate scale. The landscape setting of the development will be of high quality befitting this grade II* listed building.



Figure 3

2.2 Figures 3 and 4 provide examples of how the Council's vision could be achieved and give a flavour of the high quality design that is encouraged. These are not intended to be definitive and it is recognised that other design approaches may still meet the County Council's aspirations. Section 4 of this document lists the detailed criteria that the development must satisfy.

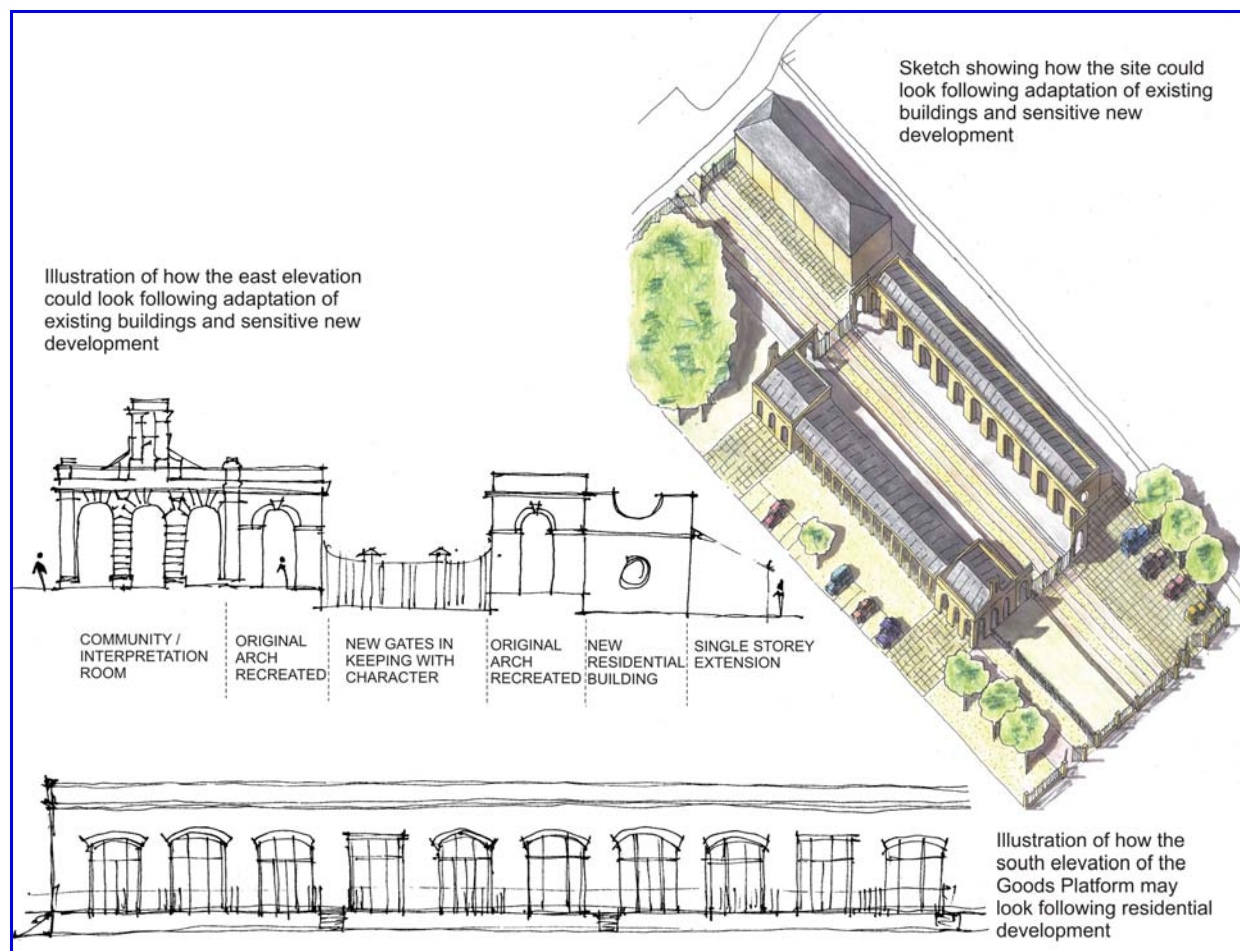


Figure 4

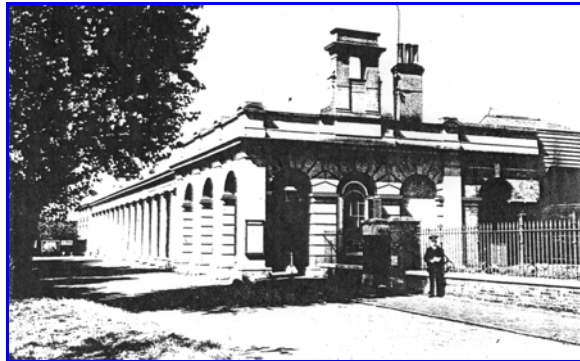
3.0 ANALYSIS

3.1 Station History

3.1.1 An historical analysis of the site has been undertaken by Gifford and Partners on behalf of the County Council and this will be made available to the preferred developer. Below is a summary providing background information to help prospective developers draw up their initial schemes.

3.1.2 Gosport Station was designed by Sir William Tite and opened in 1841 as the terminus of the London and South West Railway Company (LSWR). Shortly after opening the track was extended to Royal Clarence Yard where a new station was built to allow Queen Victoria to board her yacht directly. In 1863 the passenger platform was extended and between 1884 and 1886 a new goods platform was added to the north of the existing building. Queen Victoria's funeral procession

passed through the station in 1901. During World War II the station was hit by an incendiary bomb which led to fire destroying much of the main roof and gutting the passenger terminal. The last passenger train ran from Gosport in 1953 and the last goods train in 1969.



Gosport Station circa 1930 — Lens of Sutton

3.2 Site context

3.2.1 The site is located within the St George Barracks South Conservation Area. Immediately to the north of the site is a

footpath / cycleway that runs from Fareham to Gosport, predominantly following the former railway line. Beyond this are the backs of surrounding, predominantly residential, development. At the junction of Spring Garden Lane and Mumby Road to the north east of the site are Grade II Listed Buildings, Spring Garden Guest House and the Railway Inn. To the north west is Pearce Court, a recent (late 20th century) 3 storey residential development for the elderly which was built on the former goods yard.

3.2.2 To the west of the site is George Street, a short cul-de-sac of 1980s red brick 2 storey townhouses. This development faces the station site and therefore offers the opportunity for access to the station site.

3.2.3 To the south are the back gardens of 1920s / 30s detached and semi detached villas along Grove Avenue and Spring Garden Lane. At the south east corner of the site, the adjoining residential properties front Spring

Garden Lane, therefore presenting their side elevations to the station site. They are consequently positioned much closer to the boundary (2m).

3.2.4 The eastern approach to the site offers the only really open aspect. The main A32 (Mumby Road) cuts diagonally across the front of the site offering excellent views into the site when travelling westwards. The junction of Mumby Road and Spring Garden Lane is softened by a central triangular grassed area containing mature trees. To the east of the site is the open, former playing fields of St. George Barracks with the Gosport Town Centre just beyond.

3.3 Site character

3.3.1 The Grade II Listed railings along the eastern boundary have been restored and allow views into the site which is predominantly grassed apart from the 2 platforms, the booking hall and the northern perimeter wall. In the



Spring Garden Guest House



Road Junction



Houses on Spring Garden Lane



Pearce Court



Houses on George Street



Cycleway



south west corner there are a number of self seeded sycamores and other vegetation which make this area 'greener' than the rest of the site. Some original granite setts remain in the north east corner of this flat site and there is a short section of track although not in its original position.

3.4 Movement and access

3.4.1 The site is a few metres from Mumby Road (A32), the main route into Gosport, and less than 0.5km from the town centre, including a bus terminus and a ferry link to Portsmouth. Given the site's access to public transport and every day facilities its level of accessibility is high and this is confirmed by the Hampshire Parking Strategy and Standards.

3.4.2 The Fareham to Gosport cycle route runs along the northern boundary of the site along a surface shared with pedestrians. This provides excellent access for cyclists and therefore the detailed design for the development must

ensure that there is easy access onto this route and that provision is made for cycle parking and storage.

3.4.3 The existing access into the site is through the three sets of large iron gates in the Spring Garden Lane boundary. The whole of the eastern and northern boundaries, and part of the western boundary, are adjacent to public highway and therefore offer potential additional access points. An additional access from the west is likely as the potential impact on the listed building means it will not be acceptable for vehicular traffic to pass through the site east west. From the east vehicular access should be through the existing gates and from the west should be via the existing turning head at the end of George Street.

3.4.4 In terms of parking requirements, the objective should be to minimise its impact from public view. The provision of parking below the maximum standards will be acceptable due to the site's accessibility.

3.5 Vegetation and wildlife

3.5.1 The only area of significant vegetation is the south west corner where there are a number of sycamore trees. These are of landscape value although they are in need of maintenance.

3.5.2 The County Council commissioned an ecological study of the site, which has identified a badger sett, Japanese Knotweed, fox earths and the potential for bats, although no bats are currently roosting within the site (July 2005). Badgers are a protected species under the Protection of Badgers Act 1992 and whilst they can be moved with extreme care this must be to an area in close proximity and under licence from English Nature. The badgers sett is located in the south west corner. Given the lack of alternative locations for this sett, and the amenity value provided by the trees, it is appropriate to retain this area as open space. All works within 30m of the entrance to the sett

will require an English Nature Badgers Disturbance Licence, mitigation measures during and after construction and supervision by a competent wildlife consultant. The ecological report will be made available to potential developers.

3.6 Planning Policy

3.6.1 In order to develop this site, planning permission and listed building consent will be required. Applications should be made to Gosport Borough Council and will be determined in the light of the development plan and other material considerations. The development plan for the site is the Hampshire County Council Structure Plan (1996-2011) and the Gosport Local Plan. Other material planning considerations are Government Guidance (particularly PPS1, PPG3, PPG13, PPG15 and PPG16), the Gosport Local Plan Review, supplementary planning guidance published by Gosport Borough Council and guidance from English Heritage.

4.0 DEVELOPMENT REQUIREMENTS

4.1 Introduction

4.1.1 Schemes will be assessed against the following criteria as part of the 'design offer' process. Given the need to identify suitable land uses and areas for development, as well as more prescriptive guidance in relation to the design detail and materials, this section of the brief is divided into two. The first part considers general site wide issues, while the second looks at the specific issues relating to individual buildings.

4.2 General guidance

Use

4.2.1 The site is suitable for mixed use development which must include a community /

interpretation centre and can include some or all of the following: residential; offices; studios; workshops; live/work units; café/bar/ restaurant.

4.2.2 The community / interpretation centre must be provided by the developer and either leased on a peppercorn rent or given free of charge to a non profit making organisation to manage and maintain in the public interest.

4.2.3 If residential use is proposed this will be limited to a maximum of 14 dwellings none of which need to be affordable but which should include a variety of sizes. The provision of live/work units in addition to these residential units may be acceptable.

Movement

4.2.4 Vehicle access to the site should be from the existing gates on Spring Garden Lane and a new access from George Street.

4.2.5 Pedestrian and cycle access from the

pedestrian / cycle route is acceptable along its length.

4.2.6 The approach to car parking should minimise the visual impact of parked cars so that they do not detract from the high quality buildings. Car and cycle parking should be provided in accordance with the reduced standard in the Hampshire Parking Strategy and Standards. The standards for residential and commercial development are listed below.

Residential	Cars	Cycles	
		Long stay	Short stay
1 bed	0.5 spaces/unit	1 space/unit	1 loop/hoop
2-3 bed	1 space/unit	2 spaces/unit	1 loop/hoop
4 or more bed	1.5 spaces/unit	2 spaces/unit	1 loop/hoop

	Cars	Cycles	
		Long stay	Short stay
Office	1 space/100m ²	1 stand/150m ²	1 stand/500m ²
High tech industry	1 space/167m ²	1 stand / 250m ²	1 stand/ 500m ²
Food & drink	1 space per 7.5m ²	1 stand/40m ²	1 stand/20m ²

Form

4.2.7 The existing buildings shall be retained for sensitive adaptation including the platforms, walls and gates.

4.2.8 All construction work associated with the existing buildings should predominantly incorporate traditional and matching materials.

4.2.9 New development will be acceptable attached to the north of the goods (north) platform wall and in the north west corner fronting the pedestrian / cycleway. This must be designed to be complementary but subservient to the remaining platform wall. A contemporary design will be acceptable if designed to a high standard.

Landscape

4.2.10 The developer will be required to produce a landscape strategy that addresses the design of the hard and soft landscape for the whole of the site. This should be a succinct document that addresses the use of materials, street furniture, lighting, bollards, signage planting and boundary treatments.

4.2.11 The former track bed should be kept free from development so that people passing the site can understand and appreciate the layout of the former station. The route of the

former (three) railway lines should be depicted to further inform visitors of the site's history.

4.2.12 The south west corner of the site should be kept free from development and retained as a wildlife area with public access restricted through enclosure. A suitable habitat needs to be maintained for the existing badger sett therefore further planting and maintenance of the existing trees and shrubs is necessary.

4.2.13 The area adjacent to the Spring Garden Lane boundary should respect the form and content of the station buildings and enhance existing landscape features. Part of this area could form open space for use by the community in association with the community / interpretation room.

4.2.14 Hard surfaces should be constructed using natural materials which relate to and complement the restored buildings.

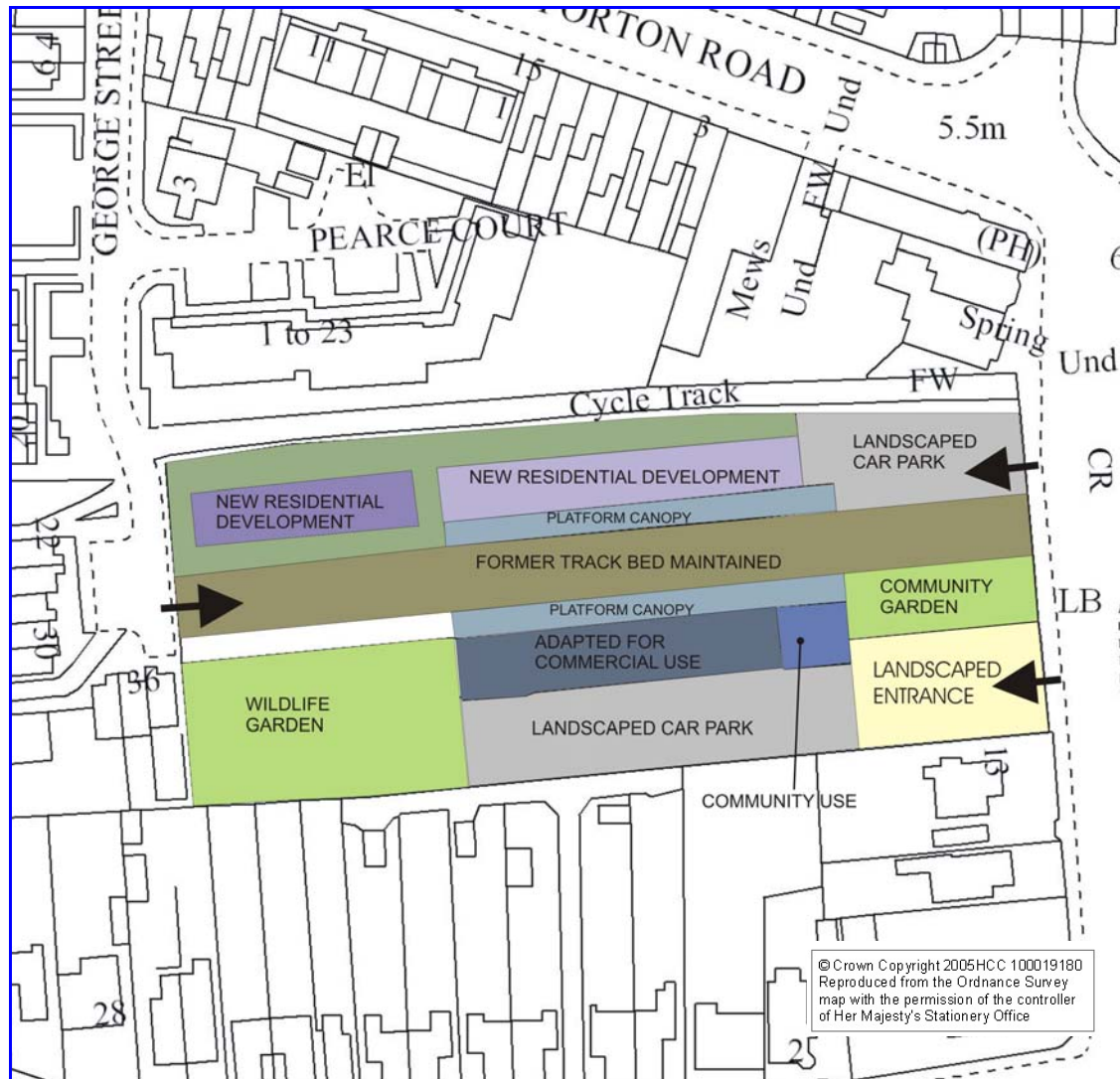


Figure 5 Layout Plan

4.3 Detailed guidance

The booking hall and stationmaster's house

4.3.1 These are the main station buildings and particular care will be needed to restore and adapt them for new uses. The eastern section should be reserved for a community use while the remainder is suitable for commercial use. Studio office accommodation seems the most appropriate and viable use, however, a restaurant, café and / or bar may also be acceptable. Residential use will not be acceptable in this building. The development must satisfy the following criteria:

- New roofs must replicate the original in terms of their visual appearance although not necessarily their construction technique. It is believed the two arcade end sections were shallow pitched slate roofs and that the central section was flat. A new roof over the colonnade should

also be provided which should be a lead monopitch with a slight slope down to the main elevation. Rainwater from the monopitch should be collected and channelled down cast iron downpipes attached to the main building façade.

- The windows will need to reflect the form and character of the original building. The prominent eastern elevation should incorporate traditional timber sash windows. Whilst traditional sash windows will be encouraged in the other elevations there is no definitive requirement. As an alternative, timber or metal framed windows may be acceptable where they respect the form and character of the original building.
- All doors must be 6 panel and constructed of solid wood.

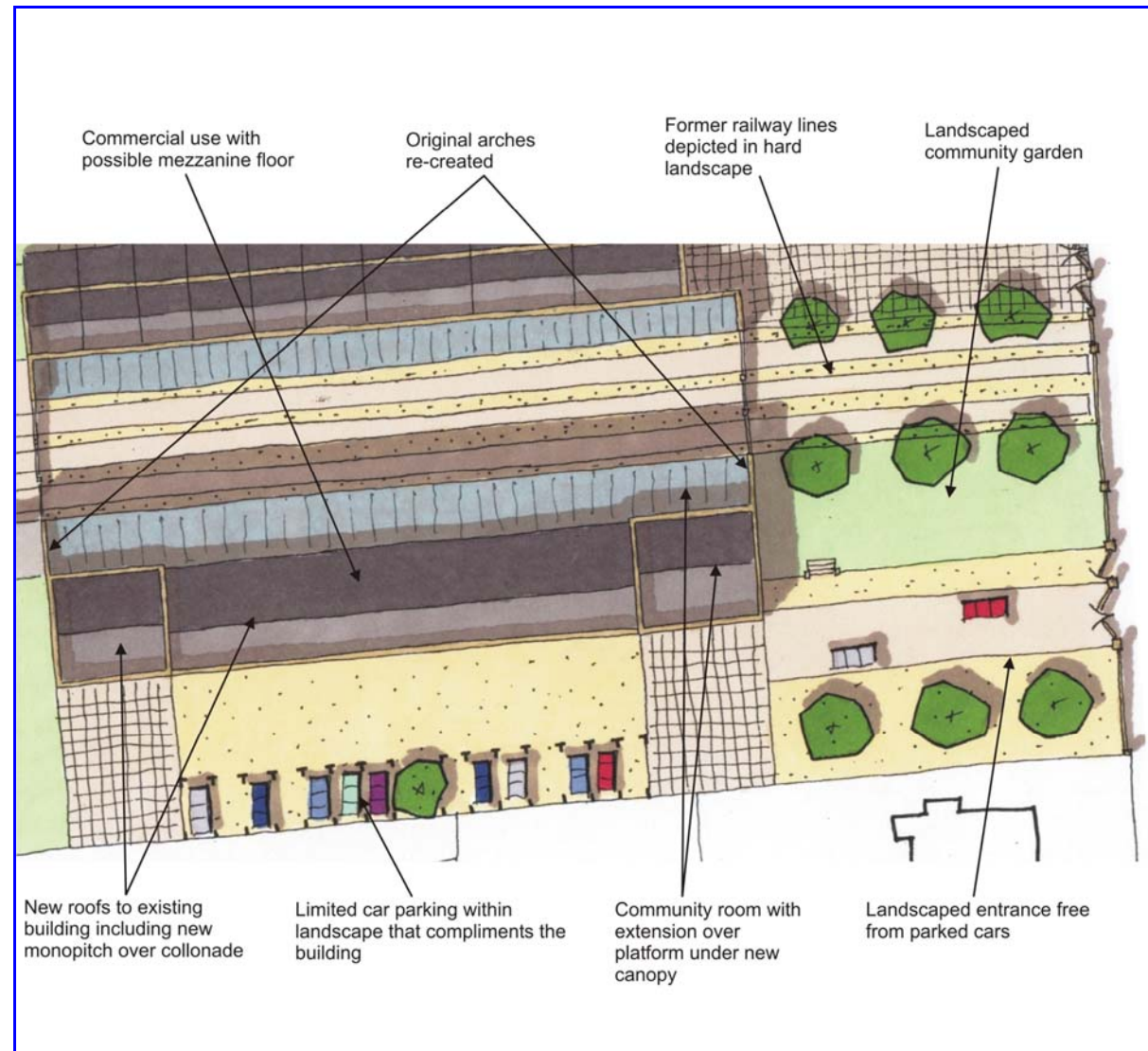


Figure 6 Booking Hall

- A mezzanine floor is acceptable in principle but must be designed not to be visually prominent through the window openings. The former stationmaster's house at the western end of the building was previously 2 storey and, therefore, the re-insertion of an additional floor would be acceptable.
- Arches at either end of the platform were an original feature of the station and should be re-created. There is no objection to a sensitively designed canopy over the platform into which the community facility could extend.

Community Room and Garden

4.3.2 As previously stated the eastern section of the building should be reserved for a community use to ensure public access to this important building. The County Council's hope

is that it will provide an interpretation centre which will inform visitors about the history of the station. The County Council and Gosport Borough Council will undertake a search for potential groups to occupy and manage this space.

4.3.3 The developer is required to sensitively adapt this part of the building and include the re-provision of the floor, an internal skin and a mezzanine floor together with toilet and kitchenette facilities. The developer will need to give careful consideration to the provision of disabled access.

4.3.4 The part of the site between the Community Room and the Spring Garden Lane boundary is to form a community garden. This should be laid out by the developer in a sensitive manner with seating and hard and soft landscaped areas. The maintenance of this could be the responsibility of the operator of the Community Room.

Goods Platform

4.3.5 The area to the north of the goods platform offers development potential for residential use. The existing station wall must be retained together with the original platform to the south. There may be potential to remove the later platform to the north of the wall provided there is no detrimental impact on its structural integrity. The development must satisfy the following criteria:

- It should front the pedestrian / cycleway to the north of the site in the form of a terrace. The remaining station wall must form an integral part of the development and the original openings must remain and be reinstated where they have been blocked up. New low level boundary treatment will be required adjacent to the footpath / cycleway appropriate to the original building. Railings are likely to be the most appropriate treatment.

- Arches at either end of the platform were an original feature of the station and should be re-created
- The new development must be set back 1 window opening from the eastern end of the platform so that it is seen as subservient to the original station.
- None of the new development shall protrude above the cornice of the remaining wall. This should allow sufficient height for a two storey building but the roof of any two storey structure should not be visible from Spring Garden Lane.
- Any two storey element of the building shall extend no more than 5 metres from the remaining station wall. A single storey element can extend beyond this to a maximum of 9 metres from the existing wall. The single storey extension must

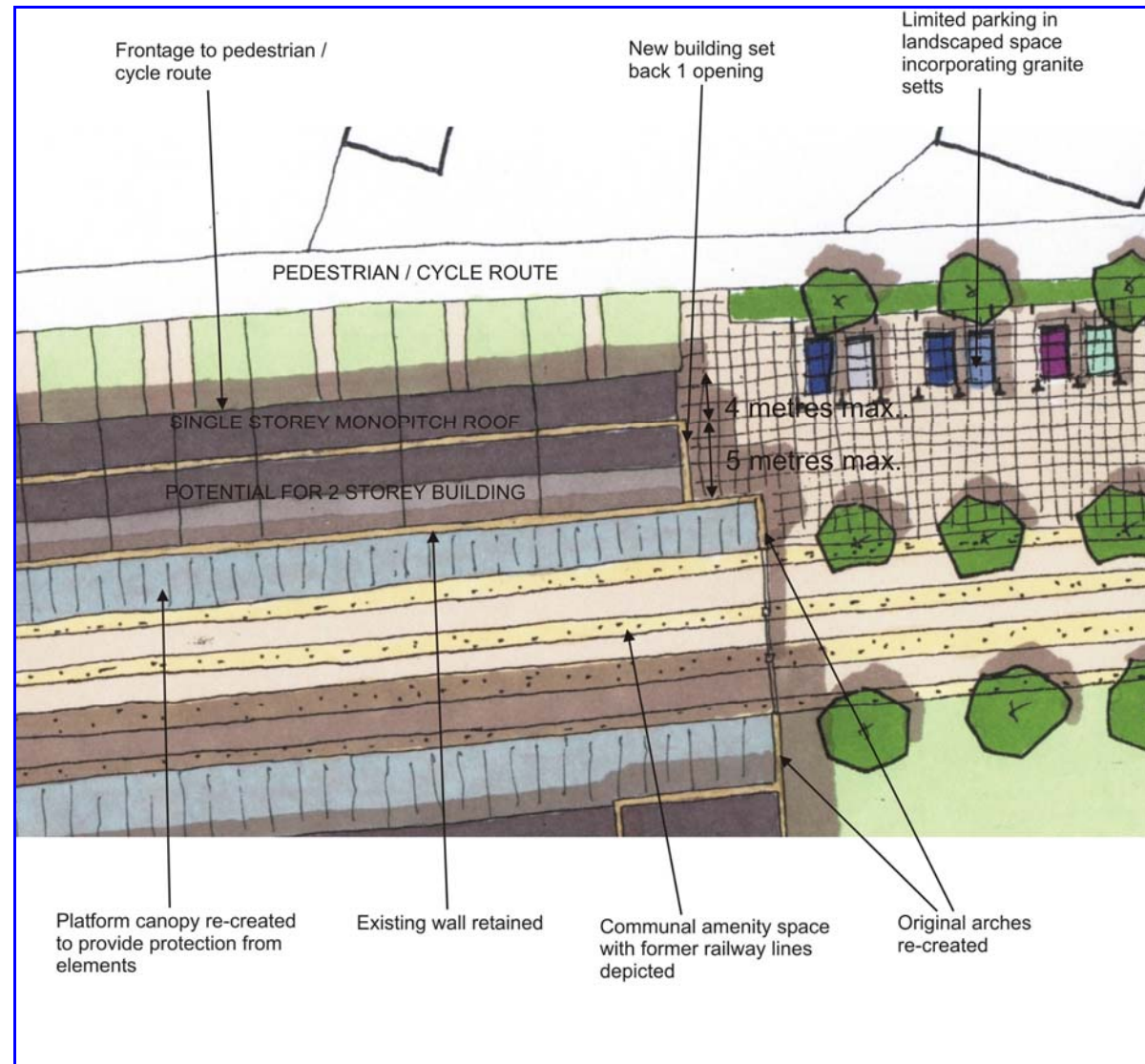


Figure 7 Goods Platform

have a monopitch roof of lead, slate or copper.

- Windows in the new development constructed in timber with side hung casements and timber 6 panel doors are likely to be most acceptable. The windows in the southern elevation of the north platform wall should occupy the existing openings but need to be recessed to the back of the remaining outer skin wall to reduce the potential for the sun to reflect off these substantial glazed areas. The windows can be metal or timber and should be consistent along this elevation.
- The remaining platform can provide attractive, south facing, private amenity space for residents. However, any subdivision needs to be subtle so that it still reads as a single platform. The provision of a canopy to protect this

space from rain and sun is acceptable in principle subject to the design details. Occasional stepped access from the platform to the former track bed will be acceptable.

- The former track bed shall remain open and be landscaped to depict the former railway lines. This will form communal amenity space for residents and therefore enclosure at either end will be acceptable providing it allows views through the site (railings or gates).
- Vehicular access for this development will be from Spring Gardens Lane through the existing gateway. Limited parking will be acceptable in the area between the gateway and the development. The granite setts that remain in this area should be reset and additional reclaimed setts used where appropriate.

New residential development

4.3.6 The area to the west of the station provides an opportunity for new build residential development. The development must respect, complement, and share some of the architectural style of the former station. A contemporary design may prove acceptable if designed to the highest specification. The development should meet the following criteria:

- The development should follow the same front and rear building lines as the proposed development of the former goods platform i.e. extending a maximum of 9 metres north from the original retained wall.
- It should be a minimum of 4 metres from the western end of the station building so it is seen as a separate development.
- It should be no more than 2 storeys high



Figure 8 New Residential Development

- with a ridge no higher than that of any development to the north of the Goods Platform.
- The development must provide a frontage to George Street.
- The windows need to be in keeping with those in the goods platform development.
- Private amenity space will be acceptable immediately to the rear of the dwellings but any subdivision needs to be subtle, as in the case of the goods platform, and not extend beyond the face of the platform.
- The surface of the route of the former railway lines should be hard surfaced with high quality paving delineating the location of the former (three) tracks in a complementary material.

Wildlife Garden

4.3.7 The south west corner of the site must be retained as a wildlife garden to protect the trees, badgers and foxes. The Badger Activity Report highlights the temporary and permanent mitigation measures the preferred developer must undertake and these are summarised below. A Badger Disturbance Licence is likely to be needed to enable construction within 30 metres of the entrance to the sett and a suitably qualified wildlife consultant should supervise work on site. Licences must be obtained from English Nature and are only valid from July to November each year.

- The Japanese Knotweed along the western boundary must be removed
- A small pond must be created as a source of water.

- The wildlife area must be fenced to prevent human encroachment
- Fruit bearing shrubs and trees such as Elder, Pear and Plum must be planted and log piles created to encourage invertebrate species both providing for the badgers foraging
- Bird and bat boxes should be provided in more mature trees
- Indigenous trees and shrubs should be planted along the whole of southern boundary to provide foraging and cover
- The fox earths must be removed under the supervision of a suitably qualified wildlife consultant
- During construction badgers will need supplementary feeding within the woodland area.
- New fox earths must be created in the wildlife area
- The storage of materials, plant and machinery should be 30 metres from the badger sett entrance unless otherwise approved by English Nature or DEFRA.

5.0 Essential Information

5.1 What to submit

5.1.1 As design quality is an essential part of the disposal procedure, developers are advised that their submissions provide sufficient information to allow an assessment of the quality. The County Council therefore requires developers submissions to include the following:

- A plan at a scale of 1:200 showing the layout of the buildings, uses, car parking areas, open space and indicative hard and soft landscape treatments;
- A sketch of the site at 1:200 showing the eastern elevation viewed from Spring Gardens Lane;
- A portfolio showing examples of similar

projects undertaken by the developer and / or architect;

- A design statement illustrating how the proposals meet the guidance set out in this brief and specifically identifying the materials and design details that will be incorporated in the scheme.

5.2 Contacts

5.2.1 For further information relating to the sale of this site or the overall project please contact the Project Manager, Gary Carroll, at the address below.

Estates
Property Business and Regulatory
Hampshire County Council
The Castle
Winchester, SO23 9DS
Tel: 01962 846568
Fax: 01962 846575
Email: gary.carroll@hants.gov.uk

5.2.2 For further information in relation to planning, design and listed building issues please contact our in house Planner / Urban Designer Richard Wilson at the above address or Rob Harper, Principal Conservation Officer at Gosport Borough Council.

Richard Wilson
Tel: 01962 846796
Fax: 01962 846705
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