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THE ASIA PACIFIC



North Strathfield Railway Station

Accessibility Upgrade

Statement of Heritage Impact

Prepared for WSP Australia Pty Limited

October 2018 – Final

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1. Introduction

1.1 Project background

The NSW Government is committed to facilitating and encouraging use of public transport, such as trains, by upgrading stations to make them more accessible, and improving interchanges around stations with other modes of transport such as buses, bicycles and cars. The Transport Access Program tranche 3 (TAP 3) is an initiative targeted at achieving compliance with the Disability Standards for Accessible Public Transport (DSAPT) Regulations across the Network.

North Strathfield Station has been identified for inclusion in TAP 3 for a precinct accessibility upgrade as it currently does not accommodate mobility impaired access to rail services, or meet key requirements of the Disability Standards for Accessible Public Transport (DSAPT) or the Commonwealth *Disability Discrimination Act 1992* (DDA).

The existing station layout does not meet the requirements of the *Commonwealth Disability Discrimination Act 1992* (DDA Act) or the *Disability Standards for Accessible Public Transport* (DSAPT). It also does not allow for equitable access to the station platforms. DDA and access compliance issues include:

- Access to North Strathfield Station is currently via stairs only and it does not have accessibility for wheelchairs;
- There is currently a lack of DDA-compliant accessible car parking spaces or 'kiss-and-ride' facilities;
- Some of the existing paths facilitating access to the station or interchange between modes are not currently compliant with DDA standards;
- The existing platforms provide irregular tactile pavers with inadequate contrast throughout the precinct; and
- Existing platform cross falls are not DDA-compliant in certain areas.

1.2 The Brief

In July 2017, EXTENT Heritage Pty Ltd was commissioned by WSP Australia Pty Limited to prepare a Statement of Heritage Impact for the proposed accessibility upgrade works (the Proposal) at North Strathfield Railway Station. The purpose of the report is to analyse the potential impacts of the Proposal on the heritage significance of the station and its associated elements.

1.3 Methodology

The methodology used in the preparation of this Statement of Heritage Impact is in accordance with the principles and definitions set out in *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance* and the *Statement of Heritage Impact Guidelines* produced by the Office of Environment and Heritage.

This Statement of Heritage Impact (SOHI) will review the relevant statutory heritage controls, assess the impact of the proposal on the subject property and make recommendations as to the level of impact.

1.4 Limitations

The site was inspected and photographed in relation to this proposal on 12 July 2018. The inspection was undertaken as a visual survey only.

The historical overview provides sufficient historical background to provide an understanding of the place in order to assess the significance and provide relevant recommendations, however, it is not intended as an exhaustive history of the site.

1.5 Authorship

The following staff members have prepared this Statement of Heritage Impact:

Tony Brassil	Senior Heritage Advisor
Eleanor Banaag	Senior Heritage Advisor

1.6 Ownership

The site is owned by RailCorp and managed by Sydney Trains.

1.7 Terminology

The terminology in this report follows definitions presented in *The Burra Charter*. Article 1 of the Burra Charter provides the following definitions:

Place means site, area, land, landscape, building or other work, group of buildings or other works, and may include components, contents, spaces and views.

Cultural significance means aesthetic, historic, scientific, social or spiritual value for past, present or future generations. Cultural significance is embodied in the place itself, its fabric, setting, use, associations, meanings, records, related places and related objects. Places may have a range of values for different individuals or groups.

Fabric means all the physical material of the place including components, fixtures, contents, and objects.

Conservation means all the processes of looking after a *place* so to retain its *cultural significance*.

Maintenance means the continuous protective care of the *fabric* and *setting* of a *place*, and is to be distinguished from repair. Repair involves restoration or reconstruction.

Preservation means maintaining the *fabric* of a *place* in its existing state and retarding deterioration.

Restoration means returning the existing *fabric* of a *place* to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material.

Reconstruction means returning the *place* to a known earlier state and is distinguished from *restoration* by the introduction of new material into the *fabric*.

Adaptation means modifying a *place* to suit the existing use or a proposed use.

Use means the functions of a place, as well as the activities and practices that may occur at the place.

Compatible use means a use that respects the *cultural significance* of a *place*. Such a use involves no, or minimal, impact on cultural significance.

Setting means the area around a *place*, which may include the visual catchment.

Related place means a place that contributes to the *cultural significance* of another place.

2. Site Identification

North Strathfield Railway Station is located between Queen Street and the rear of properties fronting George Street at North Strathfield, a suburb of Sydney. The station serves the Main Northern Line, between Strathfield and Concord railway stations. North Strathfield Station is an island station with two platforms, accessed from the south via a pedestrian footbridge connecting Queen Street and the pedestrian pathway running between Pomeroy Street and Hamilton Street East along the western side of the station.

The location of the Proposal is shown in Figure 1 and Figure 2.

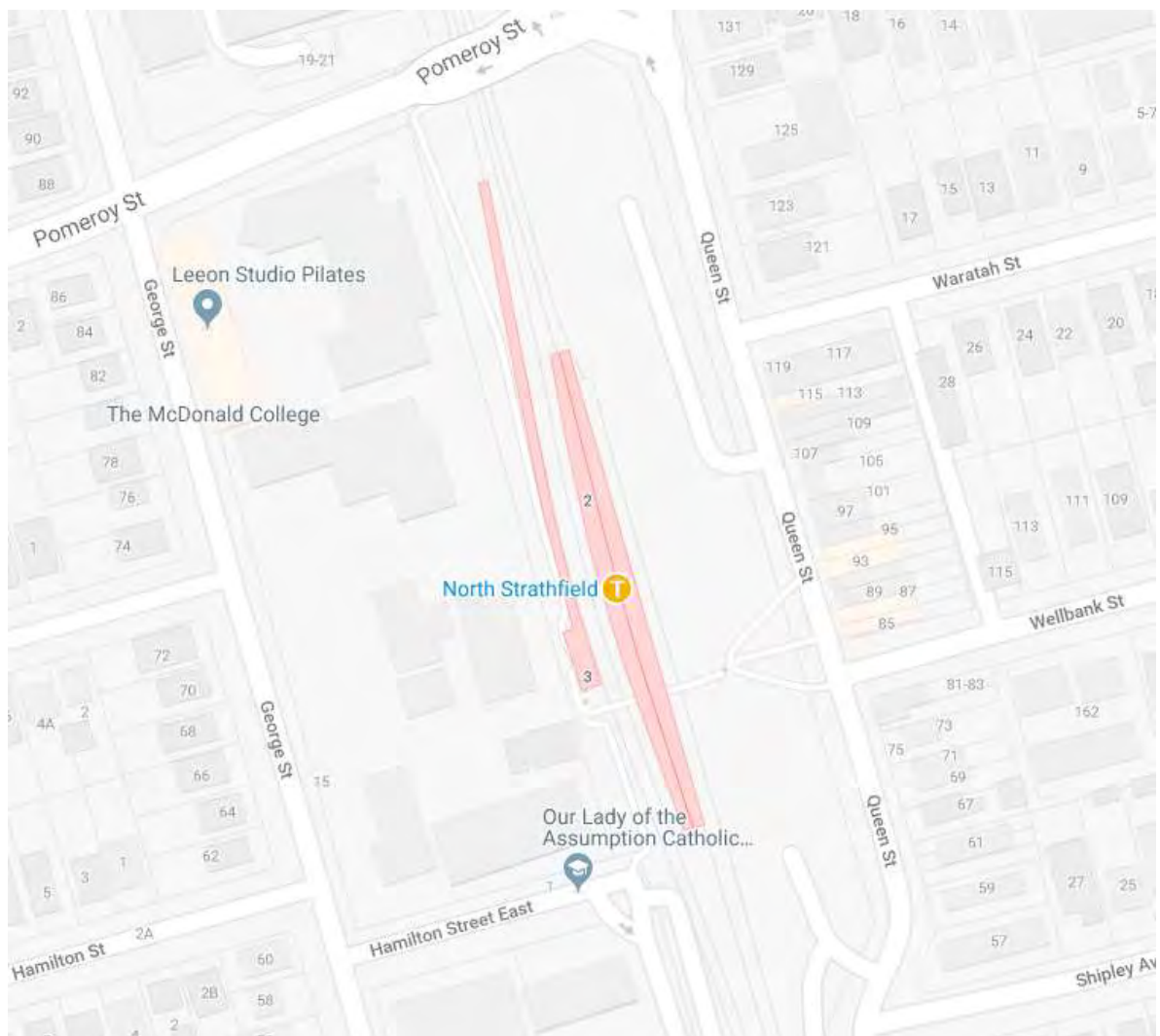


Figure 1. Location plan showing North Strathfield Station. (Source: Nearmap)



Figure 2. Aerial photograph of North Strathfield Railway. (Source: SIX LPI).

3. Historic Context

3.1 Historical Background Summary

North Strathfield Railway Station was opened on 9 June 1918, as an island platform with a standard brick station building on the platform. Access to the station was via a footbridge at the Sydney-end of the platform. A number of industrial sidings were laid in near North Strathfield Station.

In 1924, with electrification of the rail network pending, a new third platform was brought in on the Down relief line at North Strathfield with a small station building on the platform. The Down relief line and platform (Platform 3) remain in everyday service, but the Up relief line was taken out of service some years ago. No platform was ever provided on this line. The pedestrian overbridge was replaced in the late 1980s by a modern concrete beam bridge, in the same location and with the same basic arrangement.

In 2013, construction commenced on the North Strathfield Rail Underpass (NSRU) line in this vicinity, a freight line which operates parallel to the suburban passenger lines and runs alongside the North Shore Line to Hornsby. This line, at North Strathfield, was designed to dive below the existing railway triangle further south at North Strathfield and is in a cutting as it passes through North Strathfield Station. This line was completed in 2014.

3.2 Railway History

The following historic context summary is reproduced from the RailCorp Heritage and Conservation Register, as provided in the online State Heritage Inventory database:

The Main Northern line between Sydney and Newcastle was constructed in two distinct stages and in the earliest years, was worked as two separate railway systems.

The Strathfield to Hornsby section of the Northern line was opened for traffic on 17 September 1886. The line was constructed as a 'single line' and Hornsby became the temporary terminus and remained so until the extension to Hawkesbury River was opened in 1887. At the time of the opening, stations were provided at Ryde, Dundas (later Eastwood), Field of Mars (now Epping), Beecroft, Thornleigh and Hornsby.

The line between Newcastle and the northern bank of the Hawkesbury River (near present day Wondabyne) was opened in January 1888. The line was completed between Sydney and Newcastle with the opening of the massive bridge over the Hawkesbury River in 1889.

In 1891, a double track connection was laid in between the Main Northern line and the Main Western line at Strathfield / Homebush. This was called North Strathfield Junction and a signal box was built to control traffic over this line. This connection thus formed a triangular connection between the Main Northern line, Strathfield and Homebush.

The line between Strathfield and Hornsby was duplicated in March 1892.

In 1904, the first of many factories and industrial undertakings was established on land to the west of the Down Main Northern line, north of North Strathfield Junction. Over the next few years, more factories were opened, including William Arnott and Co. who opened a factory north of Strathfield Junction. More were established on the land west of the main lines in the region of Concord West during the next few years. Accordingly in 1911, a Down relief line was opened between North Strathfield Junction and Concord West. This relief line was parallel to the existing Down main line and sidings were laid to the appropriate factories off the Down relief line. In 1912, an Up relief road was also opened between Concord West and North Strathfield Junction.

North Strathfield Railway Station was opened on 9 June 1918, as an island platform with a standard brick station building on the platform. Access to the station was via a footbridge at the Sydney-end of the platform. A number of industrial sidings were laid in near North Strathfield station.

In 1924, with electrification pending, a new third platform was brought in on the Down relief line at North Strathfield with a small station building on the platform.

The Down relief line and platform remain in everyday service, but the Up relief line was taken out of service some years ago. No platform was ever provided on this line.

3.3 Concord Local History

The historical summary above reflects the railway focus of the listing and should be read in conjunction with the following information from the Concord Heritage Study. The following text is reproduced from an article by John Walmsley, (former) Chair, Archives and Local History Committee, based on notes prepared by Ron Ferguson for the former Concord Historical Society:

The struggle for a station at North Strathfield was very much a people's struggle. In the early 1900s there had been a general acknowledgment of the need for a station between Strathfield and Concord West by the NSW Railway Commissioners, but an alleged shortage of money – the cost was reckoned to be about £4000 – kept the proposal in limbo for nearly ten years. A considerable volume of correspondence flowed between Concord Council and the Commissioner on the matter, until there

was a promise in 1911 that a station would be built. In fact, as early as 1908 a local Raine & Horne real estate map designated an area on the west side of Queen Street as the 'site for future railway platform'. Subsequent real estate maps of 1912 and 1913 refer to the proposed station at North Strathfield, the earlier one going so far as to name it Waratah Station. With such a prospect in mind there was a marked increase in building along Queen Street and Wellbank Street (or Alva Street, as the western end of Wellbank was known then). This, of course, had a flow-on effect to the population density of the area, and an increase in the number of people having to walk to Concord West or Homebush Station, until the proposed station was built.

By 1915 the station had still not been started. This led to an ordinary citizen of Concord, Mr. H. J. Jaffray, calling an action meeting at his home to put greater pressure on the authorities. Twelve people attended the initial meeting on 22 September 1915. As convenor, Mr. Jaffray made a brief statement on the object of the meeting, which was to take immediate action and form a citizens' committee, consisting of residents and other parties, with a view to the earliest possible construction of a station. The committee was to be called the Concord New Station Committee, consisting of all those present, with the power to co-opt further members as it saw fit. Jaffray was elected chairman, Mr. J. Lynch, treasurer, and Mr. A. B. Travers, secretary (replaced by Mr. S. V. Larkin, 6 October 1915). Meetings were to be held once a week for ten weeks, and subsequently once a month, at Jaffray's house ('Ronston') in Nelson Street on account of lack of funds to hire an alternative meeting place.

An early spectacular demonstration of the earnestness of the campaign was a torchlight procession at 7.45 pm, Monday 29 November 1915. Headed by bands, including the Burwood Kilties Band and the Mortlake Brass Band, the procession marched from Strathfield Station to Waratah Street, where a well-attended public meeting in favour of the proposed station was held. Alderman Charles Savage, the mayor of Concord, presided and addresses were delivered by Mr. G. A. Richards, M.L.A. (who died a few days later), and other prominent local residents.

The publicity succeeded in obtaining the use of the Concord Picture Palace on Parramatta Road for a follow-up meeting open to members and the general public. This meeting was presided over by Mr. Jaffray. Among the items discussed was the support given to the project by a Mr. J. K. Greig 'as a very warm friend and adviser' (at the meeting of 16 November Mr. Greig donated ten guineas to cover expenses related to the demonstration of 29 November). It was decided that the names of owners of vacant lots in the vicinity of the proposed station be obtained from Concord Council for the purpose of increasing the number of signatures to yet another petition. Signatures were also to be sought from employees in local industries, such as Arnotts, Westinghouse Brake and Signal Co., and James Martin and Co. The final petition was used to show that more than one thousand adults would use the station on a regular basis.

Following this a deputation, consisting of the mayor and aldermen of Concord and residents of the district, was introduced to the Chief Commissioner for Railways (Mr. Harper) by Mr. G. A. Richards M.L.A. in order to push the claim for the long-promised railway station at North Strathfield. Before any representations could be made, however, the Chief Commissioner anticipated the request and stated that construction would commence before the end of the current financial year (30 June 1916). In the course of these proceedings a few small doubts were raised about the constitutionality of the Committee. Alderman Cumming, for example, pointed out that the Committee was not working according to a royal charter or an act of Parliament but, like the deputies of the third estate taking the Tennis Court Oath of 1789, was a self-constituted body. Moreover, rather than being elected by a majority of the residents, the Committee was composed, for the most part, of the very residents themselves. Not all residents, of course, had joined but membership remained opened to any person who wished to support the cause.

Work began in 1916 and progressed with very few interruptions throughout 1917. The principal one was the delay caused by the need to remove a 30-inch water main, over which there seems to have been a certain lack of communication between the Railways Commission and the Water and Sewerage Board.

Towards the middle of 1918 there remained the problem of choosing a permanent name for the new station and the form of celebration to take place on its completion. As to the choice of names – Concord, Strathfield North, North Strathfield, and Wellbank – North Strathfield was preferred on the grounds that it described most accurately the locality. The main objection to this name came from both the Railway Commissioner and Strathfield Council because of the possibility of confusion with the main station (Evening News, 11.9.1916). What seems more strange is that the more colourful 'Waratah Station' proposal of 1912 seems to have been abandoned altogether.

The Ladies Committee, which had for some time been involved in raising money for the cause, decided to hold a bazaar whose profits would go to the Concord Soldiers Aid Association. The site of the bazaar was to be the vacant land at the corner of Queen and Waratah Streets, where the first demonstration in 1915 had been held. The Commissioner was consulted on the probable date of completion of works, Mr. Bridekirke, the headmaster of Yaralla Public School (now Strathfield North Public School) gave permission for meetings to be held on the school premises, and the Repatriation Department gave permission to hold the bazaar for the Soldiers Aid provided that there were no raffles, 'chocolate wheels' or other gambling devices.

At last the station was opened 15 June 1918. Mr. R. B. Orchard, MP, was invited to open the bazaar, but declined. The Committee's second nomination was Miss Eadith Walker, who consented and not only made a donation of £25 but also 'spent money generously' at the Bazaar (Burwood Courier, 22 June 1918). The 33rd and final meeting of the now renamed North Strathfield New Station Committee was held

at Yaralla Public School 18 July 1918, its job well done, in order to wind up its affairs. Sadly the minutes of this final meeting did not make it to the minute book.



Figure 3. North Strathfield Station, c.1933, prior to its reconstruction as an Island platform. (Source Canada Bay Council)



Figure 4. Garrett Locomotive at North Strathfield Station, looking south from the vicinity of the Pomeroy St overbridge, August 1952 (Source: State Records 17420_a014_a014000239)

3.4 Subsequent Events

There are two major changes that have occurred at North Strathfield Station which are not identified in the above texts. The pedestrian overbridge was replaced in the late 1980s by a modern concrete beam bridge, in the same location and with the same basic arrangement.

In 2013, construction commenced on the NSRU line in this vicinity, a freight line which operates parallel to the suburban passenger lines and runs alongside the North Shore Line to Hornsby. This line, at North Strathfield, was arranged to dive below the existing railway triangle further south at North Strathfield and is in a cutting as it passes through North Strathfield Station. This line was completed in 2014.



Figure 5. North Strathfield Station in 2009 (left) and in 2018 (right) showing the construction of the NSRU line in the intervening period. (Source: GoogleEarth)

4. Heritage Status

4.1 Heritage Listings

The desktop search identified

Two listed heritage items within the Proposal area:

- North Strathfield Railway Station Group, which is listed on the RailCorp Section 170 Heritage and Conservation Register (item 4801029)
- 12 street trees within the landscaped garden on Queen Street, listed in Schedule 1 of the City of Canada Bay LEP 2014 heritage register
- No heritage items listed on the World, Commonwealth or National Heritage Lists, the Register of the National Estate or NSW State Heritage Register within proximity of the Proposal.

North Strathfield Railway Station is listed in the following statutory heritage registers:

Register/Listing	Item Listed	Item Name	Item Number
Statutory Registers			
National Heritage List	Not Listed	-	-
Commonwealth Heritage List	Not Listed	-	-
State Heritage Register (SHR)	Not Listed	-	-
RailCorp Section 170 Heritage and Conservation Register	Listed	<i>North Strathfield Railway Station Group</i>	4801029
Canada Bay <i>Local Environmental Plan</i> 2013	Listed	<i>Street Trees - Adj. To North Strathfield Railway Station</i>	397

North Strathfield Railway Station Group

North Strathfield Railway Station Group is listed on the RailCorp (Sydney Trains) S170 Heritage and Conservation Register (#4801029). The 'Street Trees – Adj To North Strathfield Railway Station' are listed in the Canada Bay LEP (2013) (#397).



Figure 6. North Strathfield Station, Heritage Curtilage (orange) within the railway property boundaries (red shading) (Source: SHI Database)



Figure 7. Heritage items within the vicinity of North Strathfield Station

4.2 Heritage items in the Vicinity

North Strathfield Railway Station is not in the vicinity of any other heritage items listed on the Canada Bay LEP apart from *Street Trees - Adj. To North Strathfield Railway Station (Item 397)*.



Figure 8. Heritage Map (2) from the Canada Bay Local Environmental Plan 2013. North Strathfield Railway Station is outlined in red. Heritage items are shown in Brown. Street Trees - Adj. To North Strathfield Railway Station is Item No. I-397 on this map. (Source: Canada Bay Council)

5. Physical Description

5.1 North Strathfield Station

The North Strathfield Station is comprised of an island platform (Platforms 1 and 2) and a western side platform (Platform 3). The island platform includes a single-storey, heritage listed brick building containing a station office, staff and public amenities and waiting room. The western side platform is currently not in use and contains a small stand-alone shelter to the north of the footbridge.

Station Access

The station is bound by two overhead bridges; Pomeroy Street bridge (traffic and pedestrians) to the north of the station and a footbridge to the south. The footbridge provides access (via stairs) to the station platforms. On the western side, footbridge stair access is via a long pedestrian ramp to Pomeroy Street to the north and a shorter ramp to Hamilton Street East to the south. A landscaped forecourt is located at the Queen Street access.

The footbridge was replaced in the 1990s with a concrete deck and reinforced concrete stairs, without any canopy coverage. The side platform can also be accessed through a gate adjacent to MacDonal College.

Interchange Facilities

Public transport and other interchange facilities surrounding the station include:

- ♦ An existing bus stop located on the eastern side of the station, along Queen Street to the north of Wellbank Street;
- ♦ An existing single 'kiss-and-ride' space along Queen Street to the south of Wellbank Avenue; and
- ♦ An existing single 'kiss-and-ride' space within the cul-de-sac of Hamilton Street.

5.2 Heritage Description

The following physical description is reproduced from the RailCorp Section 170 Heritage and Conservation Register, as provided in the online State Heritage Inventory database.

STATION BUILDING (1918)

Exterior: The North Strathfield station building is a single storey brick building with gabled corrugated iron roof located on an island platform (c.1918). Typical of early 20th century railway station buildings on the Short North line, it is constructed in face brick with rendered details including string courses, architraves, and window sills. Brickwork is tuck-pointed. On each side are wide corrugated metal awnings on curved cast iron brackets supported on rendered corbels which are part of engaged brick piers. There are decorative timber valances at either end of the awnings. There is a modern ticket window at the southern end of the building. Joinery is generally original including four panelled doors, coloured glass, fanlights and some double hung sash windows. A small steel-framed gabled roof extension of modern construction has been added to the southern end of the building to provide weather protection to the ticket window and platform ticket machine.

Interior: The interior of the station building features rendered walls with no dado, with evidence of the locations of former fireplaces. Some original mini-orb ceilings with metal roses are extant, along with original waratah style air vents. Much original joinery is extant. Though modified for modern use, much of the original configuration of the rooms is extant, including the former ladies waiting room with lavatories.

PLATFORMS (c1918)

The curving brick faced island platform dates to c.1918, and features a modern asphalt surface, standard modern furniture, bins and fencing. There is a long road-side platform (sic – not a platform)) on the eastern side of the station, similarly curved but of modern concrete construction.

FOOTBRIDGE (1992)

Reinforced concrete footbridge (1992).

MODERN STEEL SHELTERS (c.1990)

A small stand-alone steel shelter on Platform 3 features a skillion roof and perforated metal walls. It contains two standard issue platform benches. A small gable roofed steel frame shelter abuts the platform building at the southern end and provides shelter for a ticket machine and ticket window.

MOVEABLE

NSW Government Railways issue clock (no. 1846) in ticket office, manufactured by Seth Thomas Clock Company, Thomaston, Conn., USA. Old safe in former ticket office (no manufacturer's plate).

LANDSCAPE

Landscaped park and ornamental garden fronting Queen Street with pathways, garden furniture, small shrubbery and two rows of brushbox.

5.3 Condition

The following comments upon physical condition from September, 2008 are reproduced from the RailCorp Heritage and Conservation Register, as provided in the online State Heritage Inventory database:

- *Station Building (1918) - Good condition. The station building is generally in good condition, although the interiors need repainting. Interior walls appear to have been patched in places in preparation for repainting, but this has not been carried out.*
- *Platform - Good Condition*
- *Footbridge (1992) - Good Condition*
- *Moveable*
- *Landscape - Good Condition*

Generally, these condition assessments have been reviewed as part of the current project and have been determined to be generally correct. The interiors have been repainted. The Clock is in good condition and the Safe is in fair overall condition, although missing its nameplate, as described.

5.4 Setting and Views

The following comments upon setting and views are reproduced from the RailCorp Heritage and Conservation Register, as provided in the online State Heritage Inventory database:

CONTEXT

North Strathfield Railway Station is set in an open setting with early twentieth century suburban development to the east and former industrial estates to the west. The station contains three platforms, station building, footbridge and railway tracks associated with former sidings. A small ornamental garden marks the eastern entry to the station which is accessed by a modern precast concrete footbridge and adds greatly to the suburban setting of Queen Street. A well maintained early twentieth century island platform and building are the main features of the station.



Figure 9. North Strathfield Railway Station, viewed from the pedestrian bridge



Figure 10. North Strathfield Railway Station - northern end of platform building



Figure 11. The garden entrance to North Strathfield Railway Station from Queen St



Figure 12. The pathway from Hamilton Street to North Strathfield Railway Station, below the white canopy roof carried on white posts.



Figure 13. The non-original overhead pedestrian footbridge to North Strathfield Railway Station.



Figure 14. Platform 3 at North Strathfield Railway Station has no heritage buildings or structures

6. Assessment of Significance

Assessment of Significance – a publication developed by the Heritage Office and former NSW Department of Urban Affairs and Planning provides the basis for an assessment of heritage significance of an item or place. This is achieved by evaluating the place or items significance in reference to specific criteria, which can be applied at a national, state or local level.¹ The significance of the study area is assessed against the criteria below.

6.1 Assessment Against Criteria

Criterion (a) An item is important in the course, or pattern, of NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (b) An item has strong or special association with the life or works of a person, or group of persons, of importance in NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (c) An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area);

Criterion (d) An item has strong or special association with a particular community or cultural group in NSW (or the local area) for social, cultural or spiritual reasons;

Criterion (e) An item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (f) An item possesses uncommon, rare or endangered aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (g) An item is important in demonstrating the principal characteristics of a class of NSW's cultural or natural places; or cultural or natural environments. (or a class of the local area's cultural or natural places; or cultural or natural environments.)

The following assessment of significance is provided in the State Heritage Inventory database for *North Strathfield Railway Station*.

¹ NSW Heritage Office & Department of Urban Affairs and Planning (DUAP) 1996, *NSW Heritage Manual*, NSW Heritage Office and DUAP, Sydney.

Criteria	Assessment of Significance
<p>SHR Criteria a) [Historical significance]</p>	<p><i>North Strathfield Railway Station has historical significance at a local level. Although the main line was opened some years earlier, North Strathfield station was not opened until 1918, thus making it one of the last stations to be opened on the Short North line. In an area that had become home to many factories during the early part of the twentieth century, the establishment of the station encouraged the rapid subdivision and development of the area, particularly to the east of the train line.</i></p>
<p>SHR Criteria c) [Aesthetic significance]</p>	<p><i>The site has a moderate degree of aesthetic significance at a local level. The building is an example of early twentieth century railway station design with fabric and details typical of this period and is similar to other rail buildings of the late nineteenth and early twentieth century in the Sydney region. As a relatively late example of its type, the station is a good example of the consistency of railway station design that was employed on the Short North line for a period of almost 30 years.</i></p>
<p>SHR Criteria d) [Social significance]</p>	<p><i>The place has the potential to contribute to the local community's sense of place, and can provide a connection to the local community's past.</i></p>
<p>SHR Criteria f) [Rarity]</p>	<p><i>North Strathfield station complex is a common station type, well represented elsewhere in the Sydney metro network, although it does contain some significant internal early fabric including a pair of ticket windows with original timber architraves and copper coin trays which are in themselves considered to be rare as the only extant examples of their type on the Short North line.</i></p>
<p>SHR Criteria g) [Representativeness]</p>	<p><i>The platform building, island platform and footbridge are representative of structures built at Sydney railway stations between 1892 and 1929, based on standard designs. The platform building has had only relatively minor alterations made to it during its lifetime and is a good example of its type. Both the platform and the footbridge are not outstanding examples of their type.</i></p>
<p>Integrity/Intactness:</p>	<p><i>North Strathfield railway station is a relatively intact example of an early twentieth century railway station, with the station platform building in particular displaying a high level of intactness and integrity, despite some minor internal alterations.</i></p>

6.2 Statement of Significance

The following Statement of Significance for *North Strathfield Railway Station* has been reproduced from the RailCorp Heritage and Conservation Register, as provided in the online State Heritage Inventory database:

North Strathfield Railway Station has local heritage significance as the establishment of the station in 1918 encouraged the rapid subdivision and development of the area, particularly to the east of the train line. The station is a highly intact, good example of the standard type railway station that was installed along the Short North line during the late 19th and early 20th centuries. The station building contains some significant internal early fabric including a pair of ticket windows with original timber architraves and copper coin trays which are rare as the only known examples of their type extant on the Short North line.

The following Statement of Significance for *Street Trees - Adj. To North Strathfield Railway Station* has been reproduced from the *Concord Heritage Study Review, 1998* prepared by Perumal Murphy Pty Ltd:

Characteristic street planting of row of brush box from c.1940s forming significant element in streetscape.

6.3 Gradings of Significance

Graded levels of significance are a management tool used to assess the relative significance of elements within an item, place or site and to assist in decision-making regarding elements of a place. The gradings of significance that have been used for elements within the study area are based on guidelines established in the NSW Heritage Division publication, *Assessing Heritage Significance*.

Table 1. Gradings of Significance Definitions²

Grading	Justification	Status
EXCEPTIONAL	Rare or outstanding element contributing to an item's local or significance.	Fulfils criteria for directly State listing. local and State
HIGH	High degree of original fabric. Demonstrates a key element of the item's significance. Alterations do not detract from significance.	Fulfils criteria for local or State listing.
MODERATE	Altered or modified elements. Elements with little heritage value, but which contribute to the overall significance of the item.	Fulfils criteria for local or State listing.
LITTLE	Alterations detract from significance. Difficult to interpret.	Does not fulfil criteria for local or State listing.
INTRUSIVE	Damaging to the item's criteria for local or State listing.	Does not fulfil heritage significance.

² NSW Heritage Office & Department of Urban Affairs and Planning (DUAP) (2001). *Assessing Heritage Significance*.

Based on the above definitions, the following gradings can be applied to individual elements on site.

Table 2. Gradings of significance for North Strathfield Railway Station

Element	Grading
Station building (1918)	HIGH
Platforms (c1918)	HIGH
Footbridge (1992)	LITTLE
Modern Steel Shelters (c.1990)	LITTLE
Moveable	MODERATE
Landscape	LITTLE

The above gradings demonstrate that while the individual items are not particularly significant or exemplary in their individual functions, they offer a contribution to the collection of steam locomotive facilities, which presently is for the most part interpretive, but some elements are still operational.

7. Proposed Works

7.1 Key features

The Transport Access Program is an initiative to provide a better experience for public transport customers by delivering accessible, modern, secure and integrated transport infrastructure. Key benefits include:

- ◆ Stations that are accessible to people with a disability, limited mobility and parents with prams
- ◆ Modern buildings and facilities for all modes that meet the needs of a growing population
- ◆ Modern interchanges that support an integrated network and allow seamless transfers between all modes for all customers.

The Proposal is intended to provide safe and equitable access between the island platform and the surrounding pedestrian pathway network at North Strathfield. The improvements are intended to provide an improved customer experience for existing and future users of North Strathfield Station.

The key features of this version of the Proposal are:

- ◆ Platform regrading and improvements in accessible paths and ramps.
- ◆ Provision of a lift to access each of the three platforms from the existing footbridge.
- ◆ Altering the height of the 'waiting room' floor to provide level access, noting that this may be removed from the design in consultation with Sydney Trains.
- ◆ Modification of the existing unisex toilet into an ambulant unisex toilet by adding grab-rails.
- ◆ Modifications to the existing station building including:
Upgrade/reconfiguration of existing FAT for DDA compliance; New SSER room; New staff ambulant toilet
- ◆ Installation of new combined services route to new SSER room within platform building
- ◆ Wayfinding (signage) modifications to help people with limited mobility navigate more easily around the station.
- ◆ Pedestrian road safety improvements in the form of:
 - Widened footpaths at the Queen Street entrance and added tactile ground surface indicators (TGSIs) to these footpaths.
 - Regrade and widening of the footpath and a new kerb ramp for DDA parking at the Hamilton Street entrance.
 - Added TGSIs on the existing footpath at the Hamilton Street entrance.
 - Provision of one new DDA-compliant on-street parking space close to the station entrance on Queen Street.

The Proposal is shown in the following Figures 15 to 30.

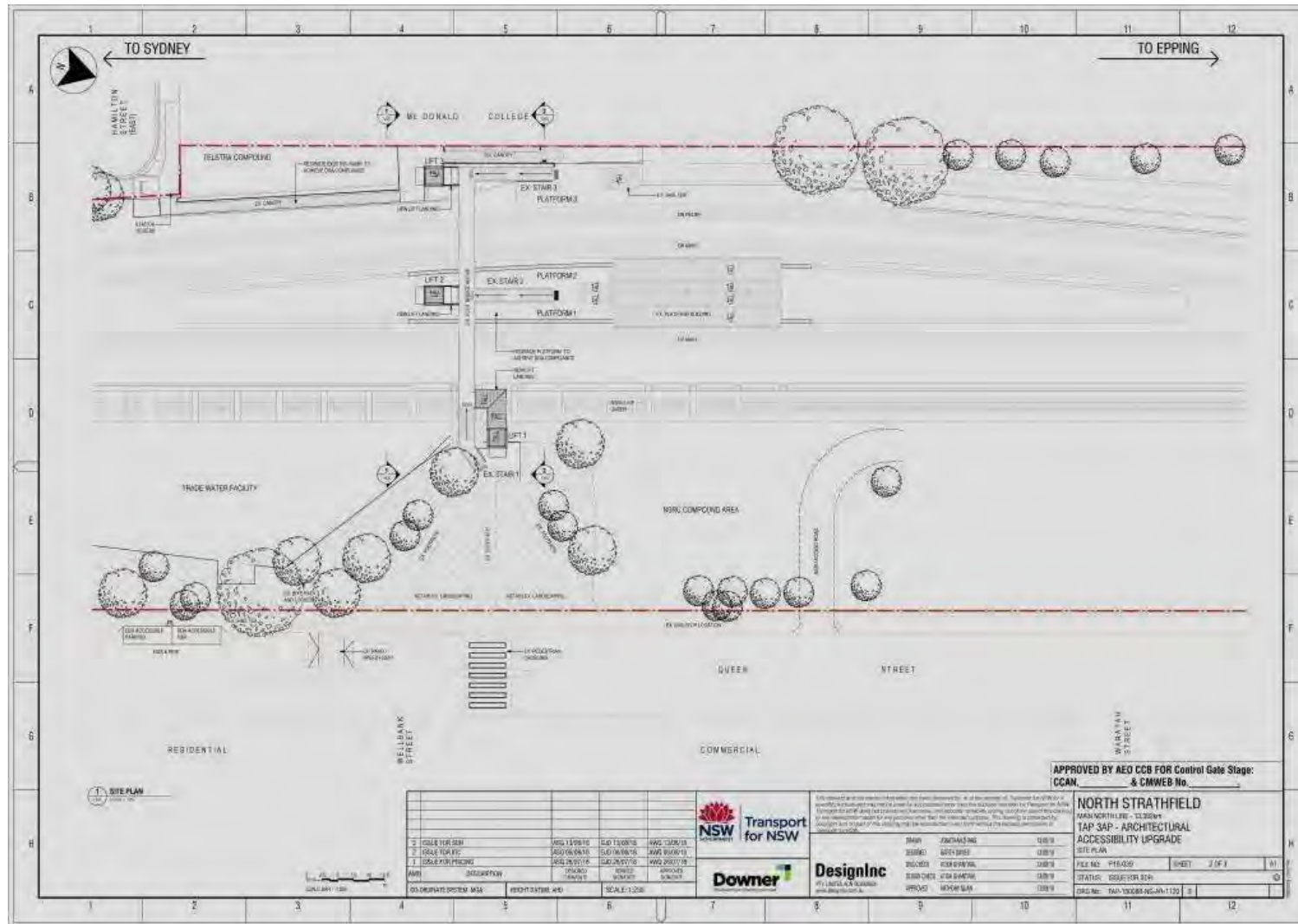


Figure 15. Plan indicating the overall scope of works (areas shown between red lines). (Source: Transport for NSW)

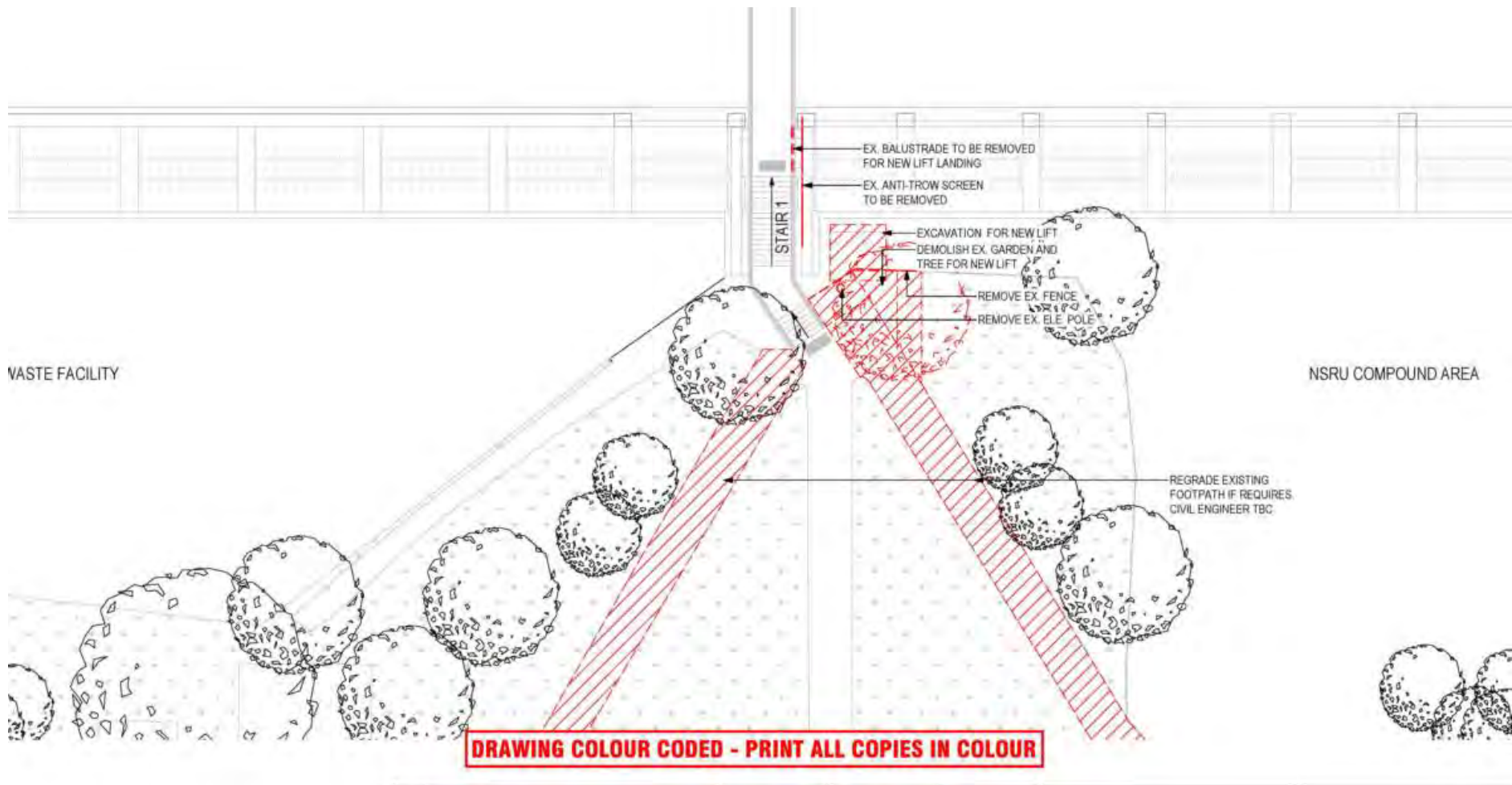


Figure 16. Plan indicating scope of demolition and excavation works around the Queen St entrance to the Station (red shading). (Source: Transport for NSW)

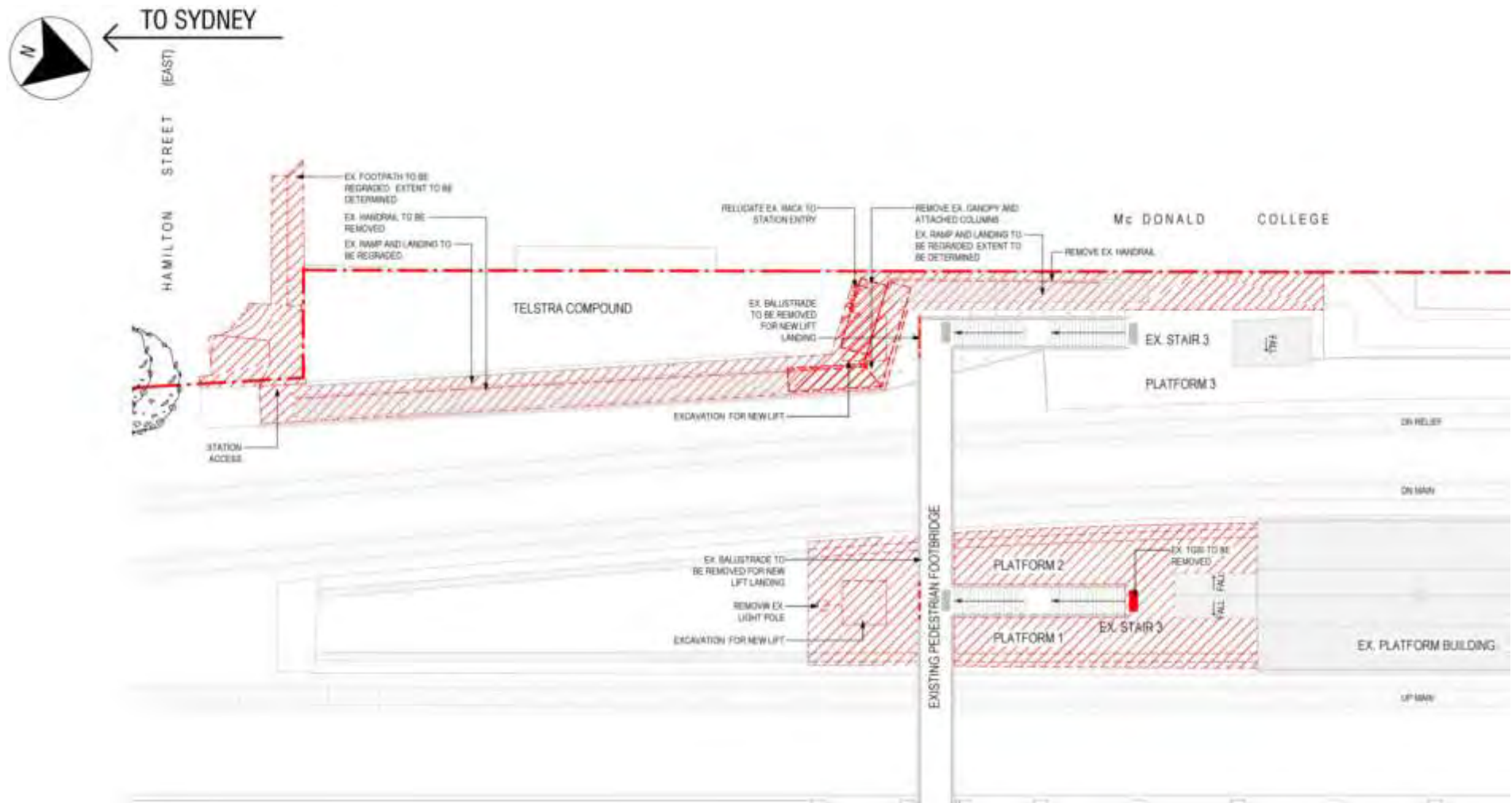


Figure 17. Plan indicating scope of demolition and excavation works (red shading) on the western approach to the Station – Works here relate to path widths, grades, canopies, balustrades and the excavations for the new lift. (Source: Transport for NSW)

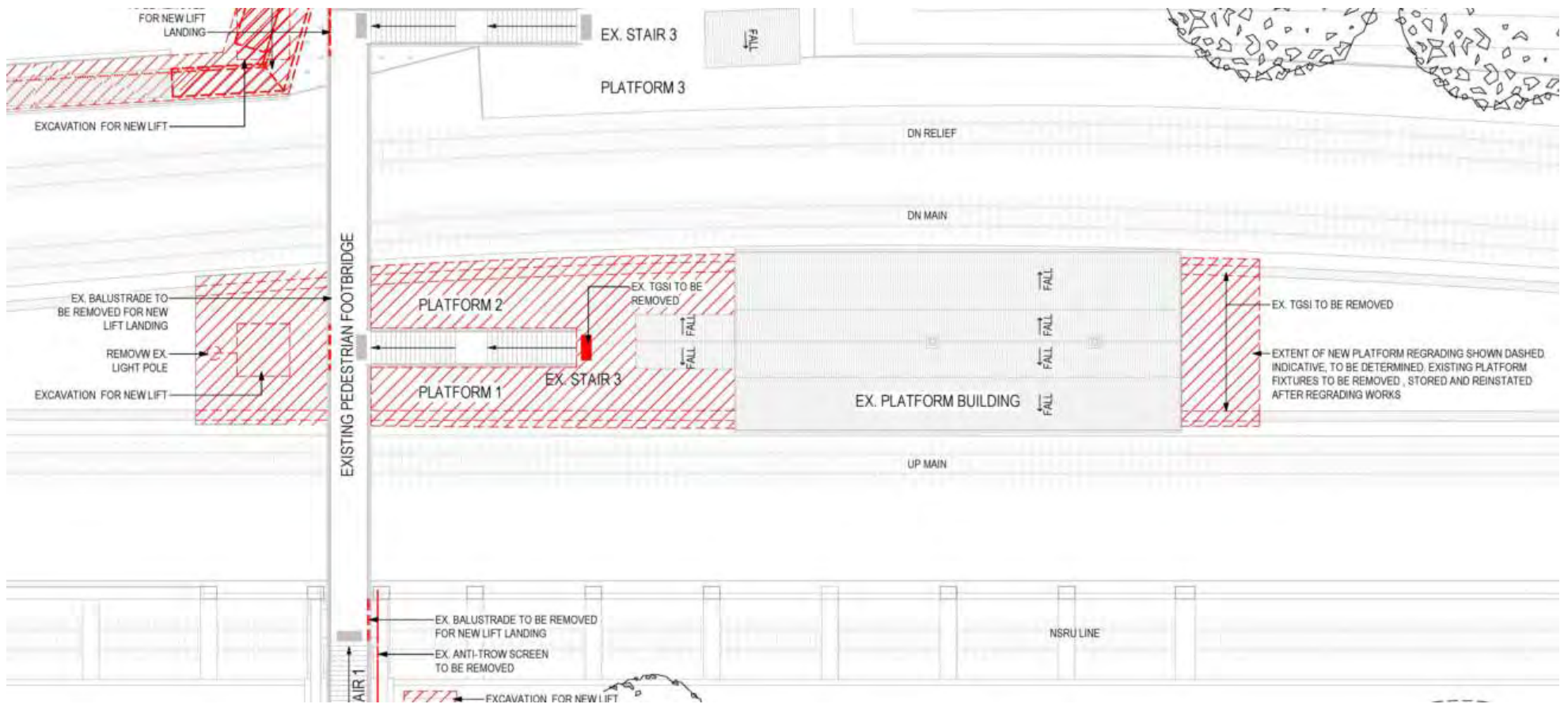


Figure 18. Plan indicating scope of demolition and excavation works (red shading) on the island platform of the Station – Works here relate to platform grades and TGS1 removals. (Source: Transport for NSW)

EXTENT

HERITAGE ADVISORS

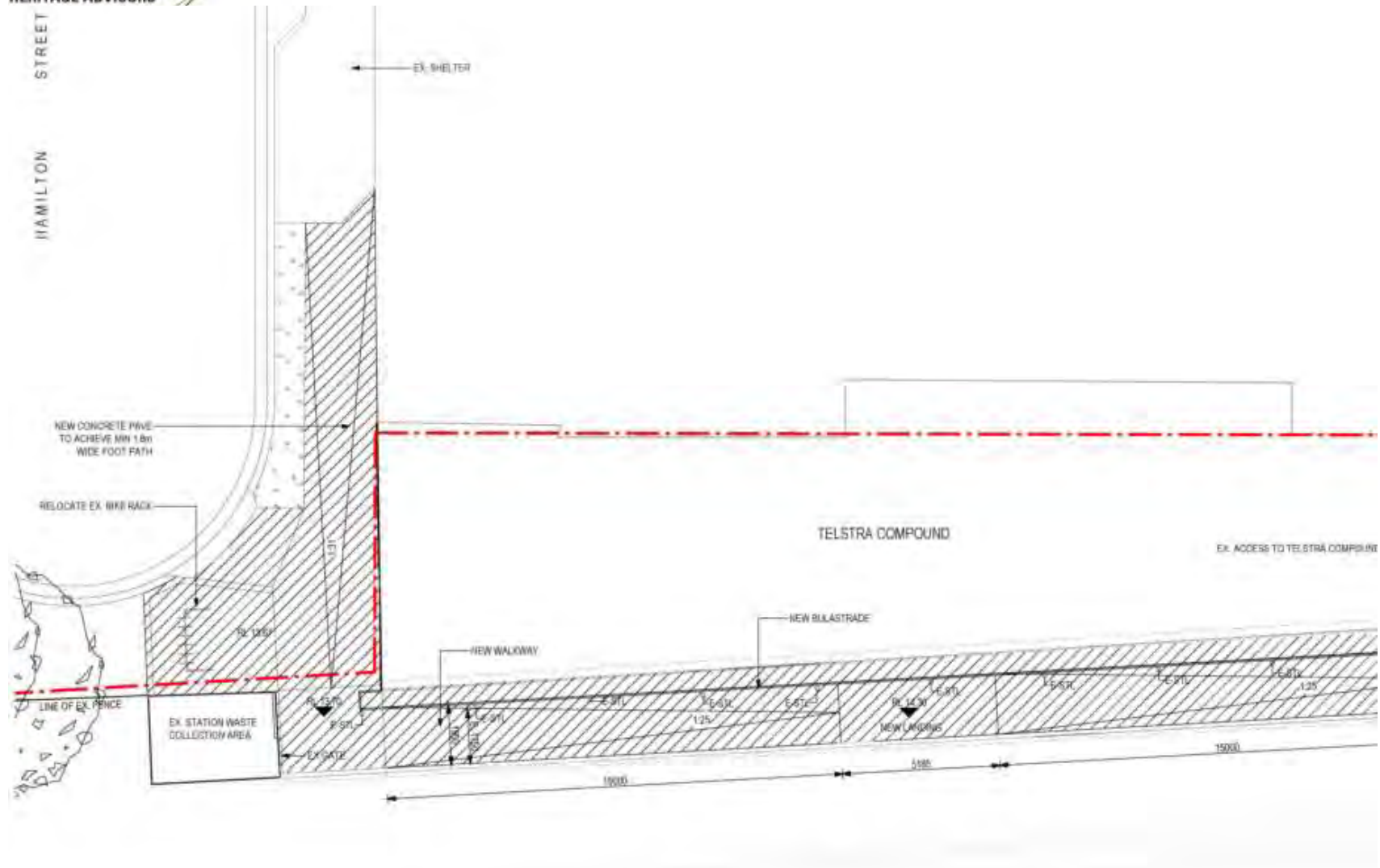


Figure 19. Plan indicating scope of works (hatched) on the Hamilton Street entry to the Station – see following Figure for continuation northwards. (Source: Transport for NSW)

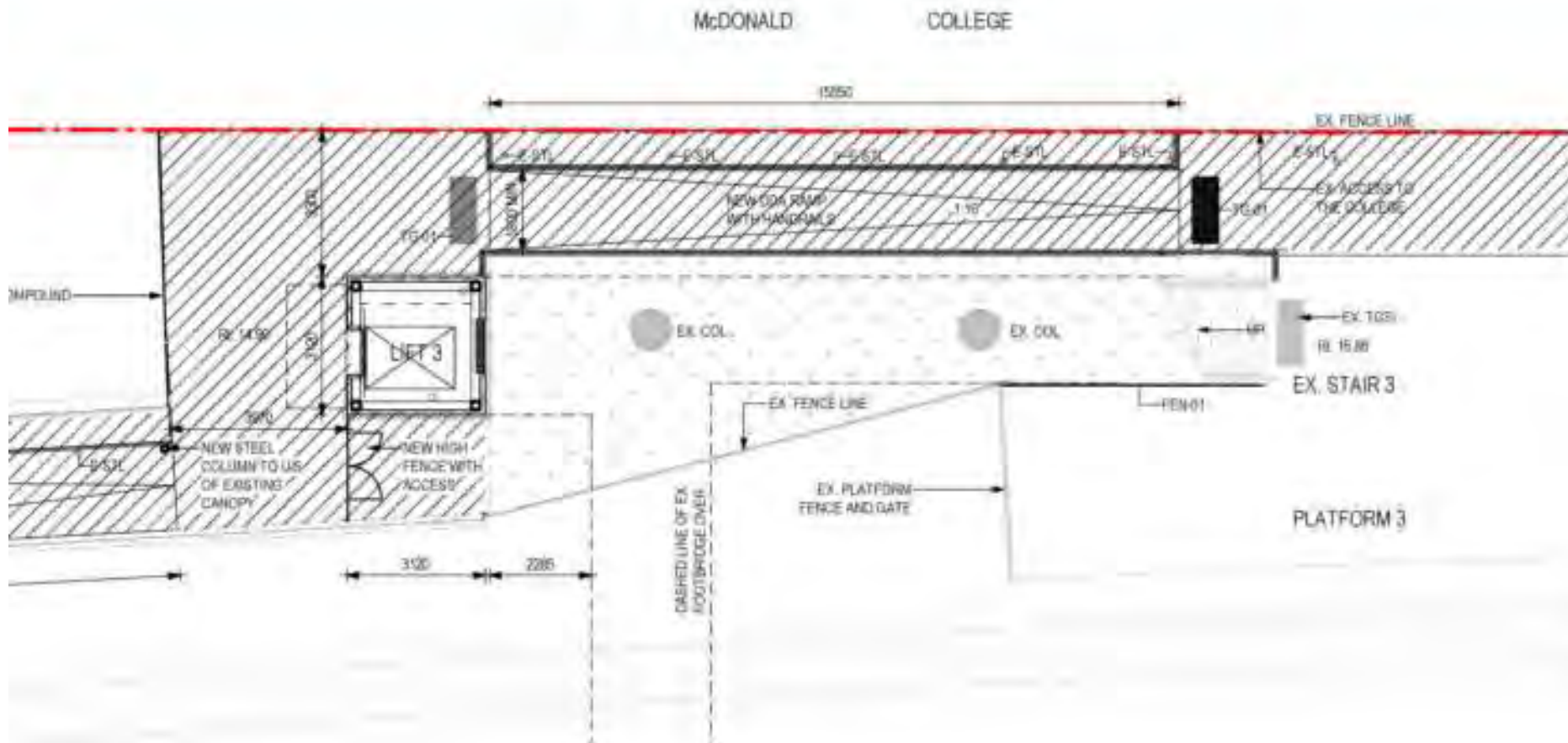
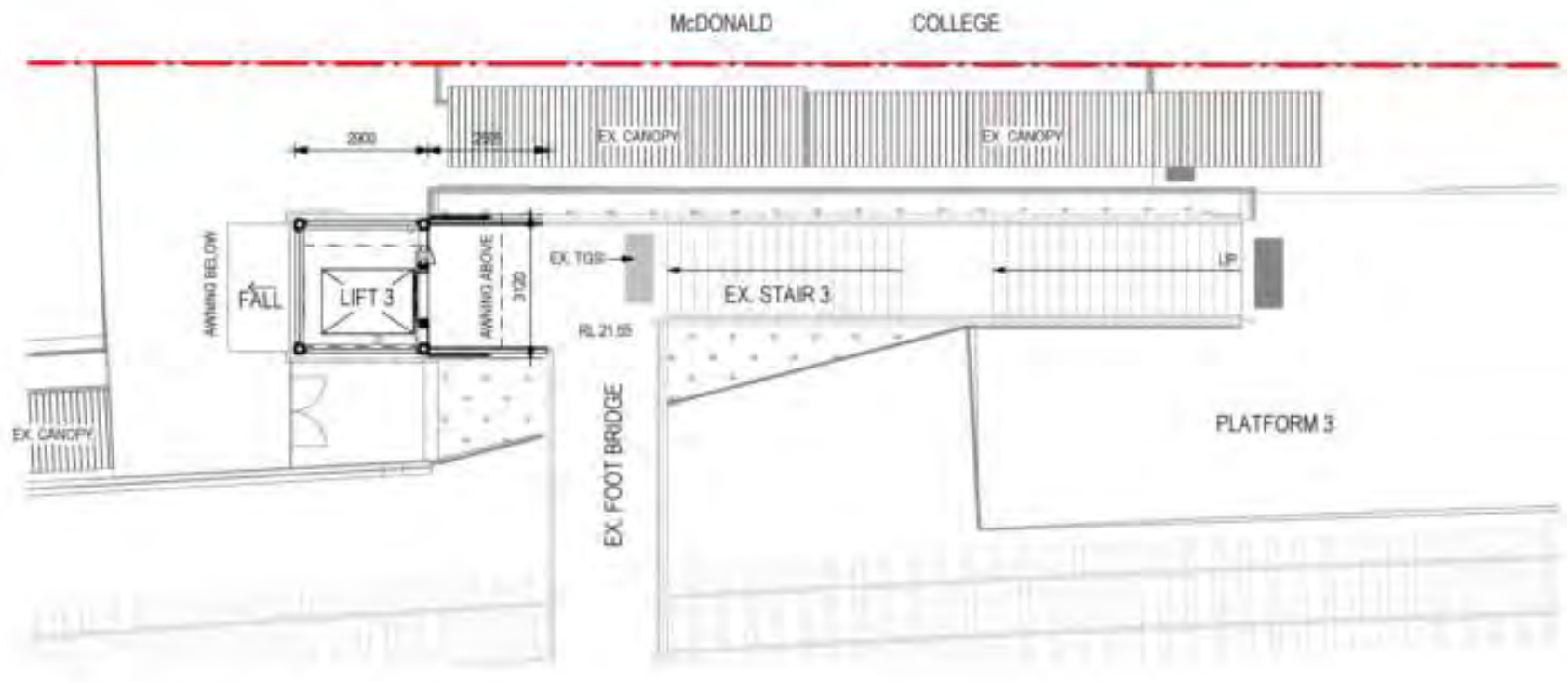


Figure 20. Plan indicating scope of works (hatched) on the Hamilton Street entry to the Station – see preceding Figure for continuation southwards. (Source: Transport for NSW)

TO EPPING →



2 PLATFORM 3 ACCESS - BRIDGE LEVEL
END SCALE 1:100

Figure 21. Plan indicating scope of works (hatched) on the Hamilton Street entry to the Station – this plan shows works at Overhead Bridge level. (Source: Transport for NSW)

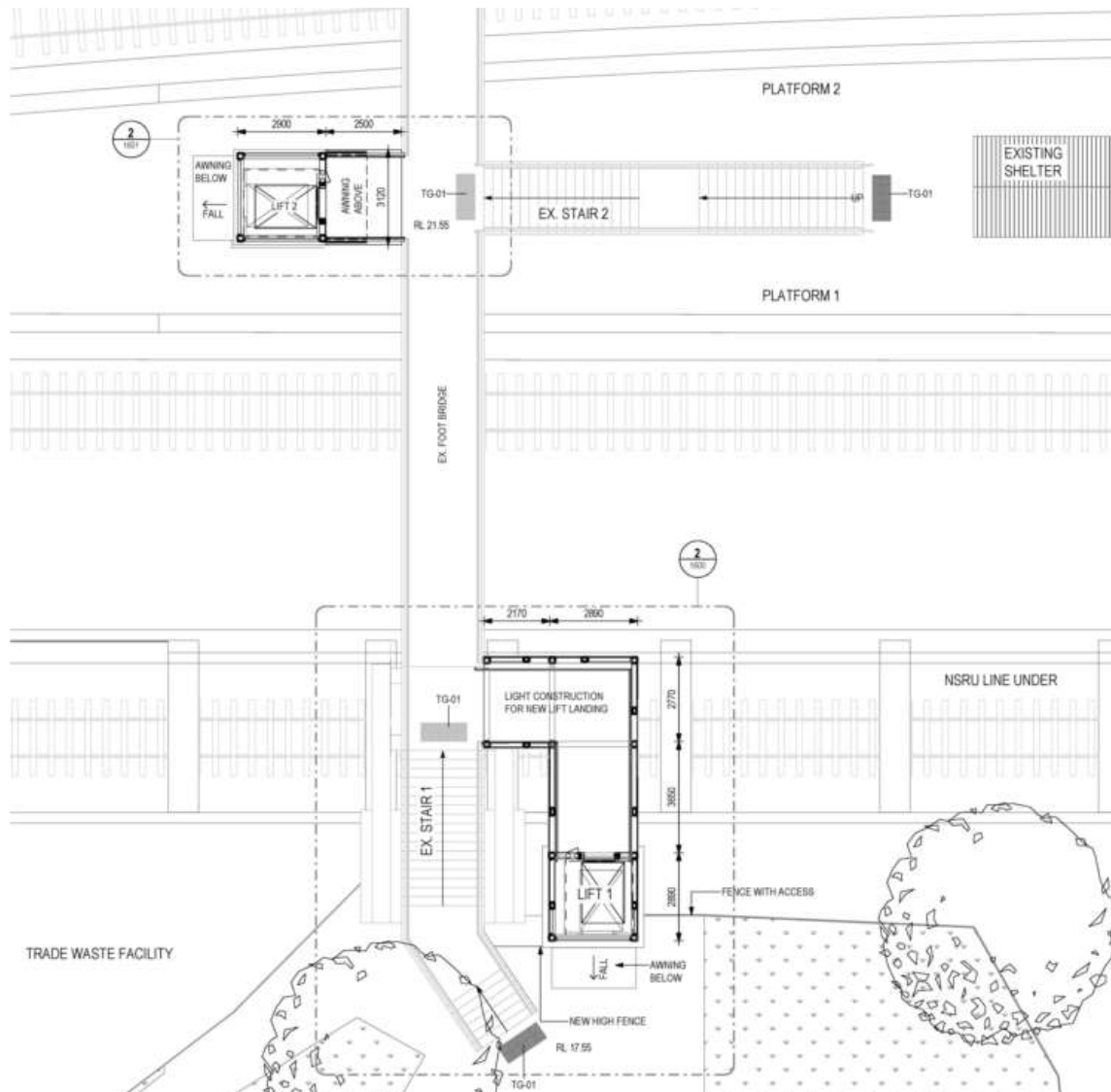


Figure 22. Plan indicating scope of works (hatched) on the Overhead Bridge level. (Source: Transport for NSW)

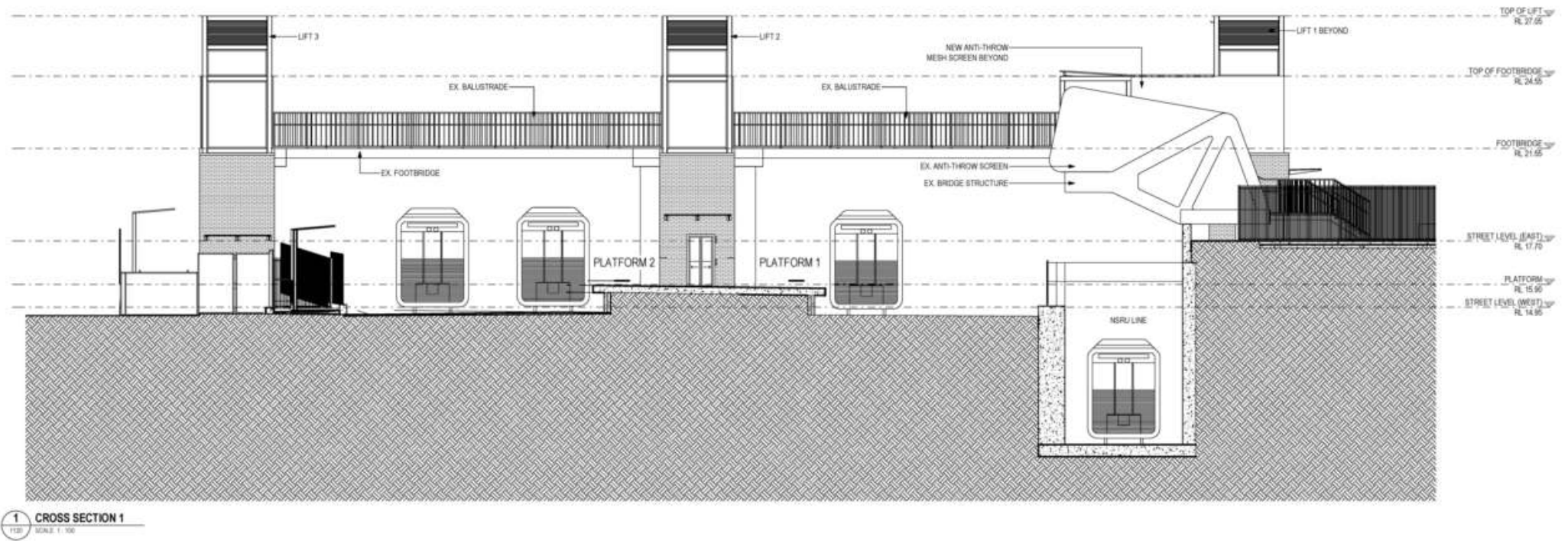


Figure 23. Elevation – view northwards (Source: Transport for NSW)

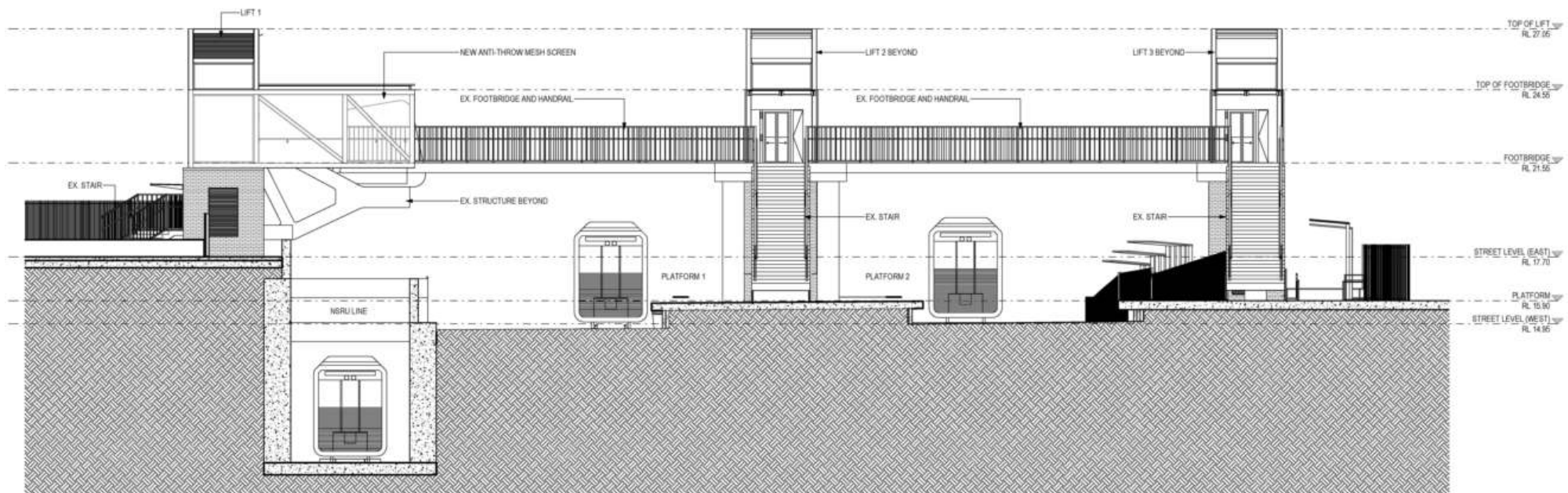
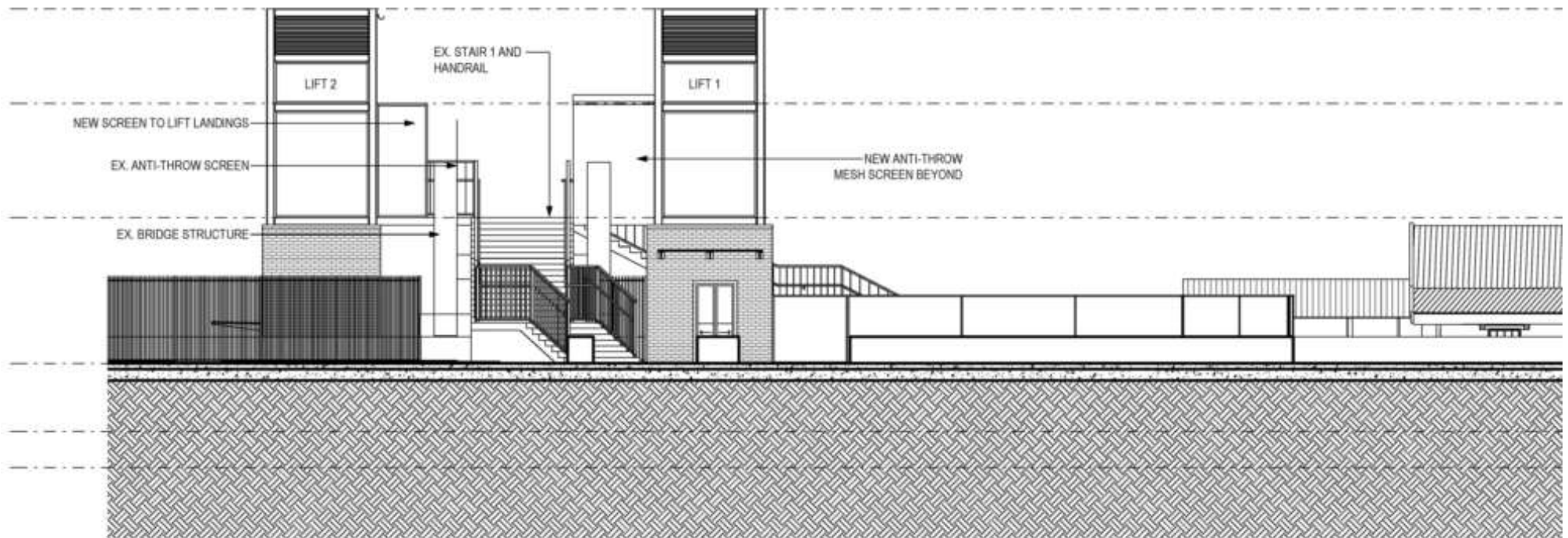


Figure 24. Elevation – view southwards (Source: Transport for NSW)



2 EAST ELEVATION - QUEEN STREET ENTRY
SCALE 1 : 100

Figure 25. Elevation – view westwards from Queen Street (Source: Transport for NSW)

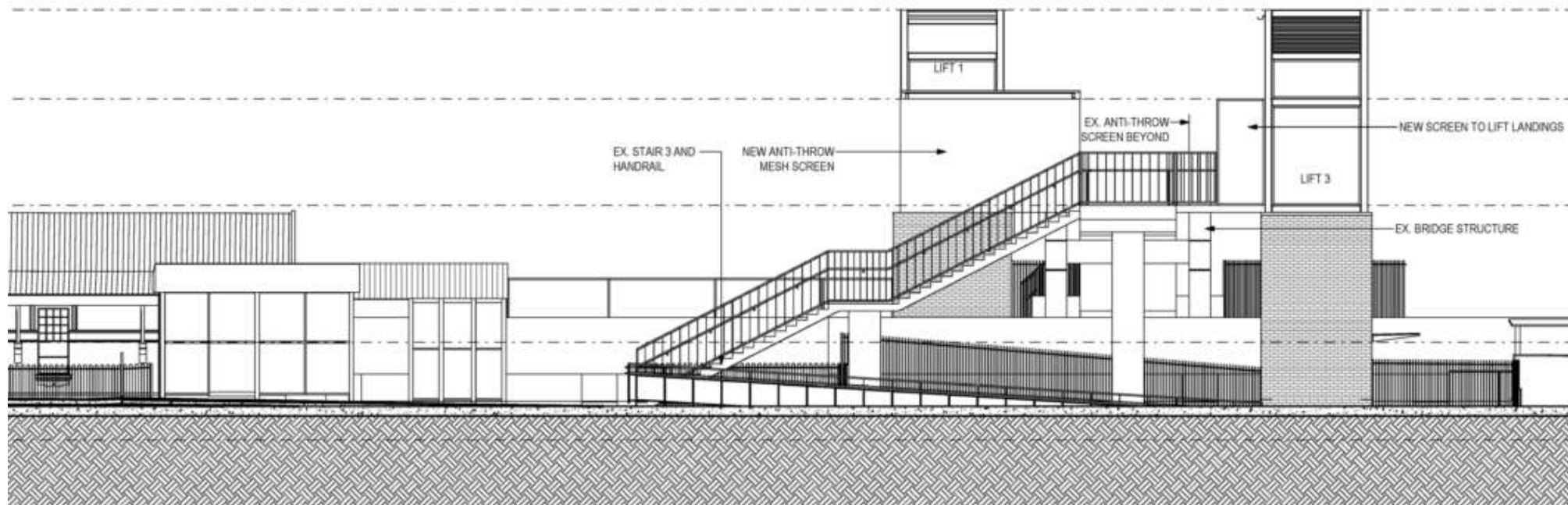


Figure 26. Elevation – view eastwards from Hamilton Street entry (Source: Transport for NSW)

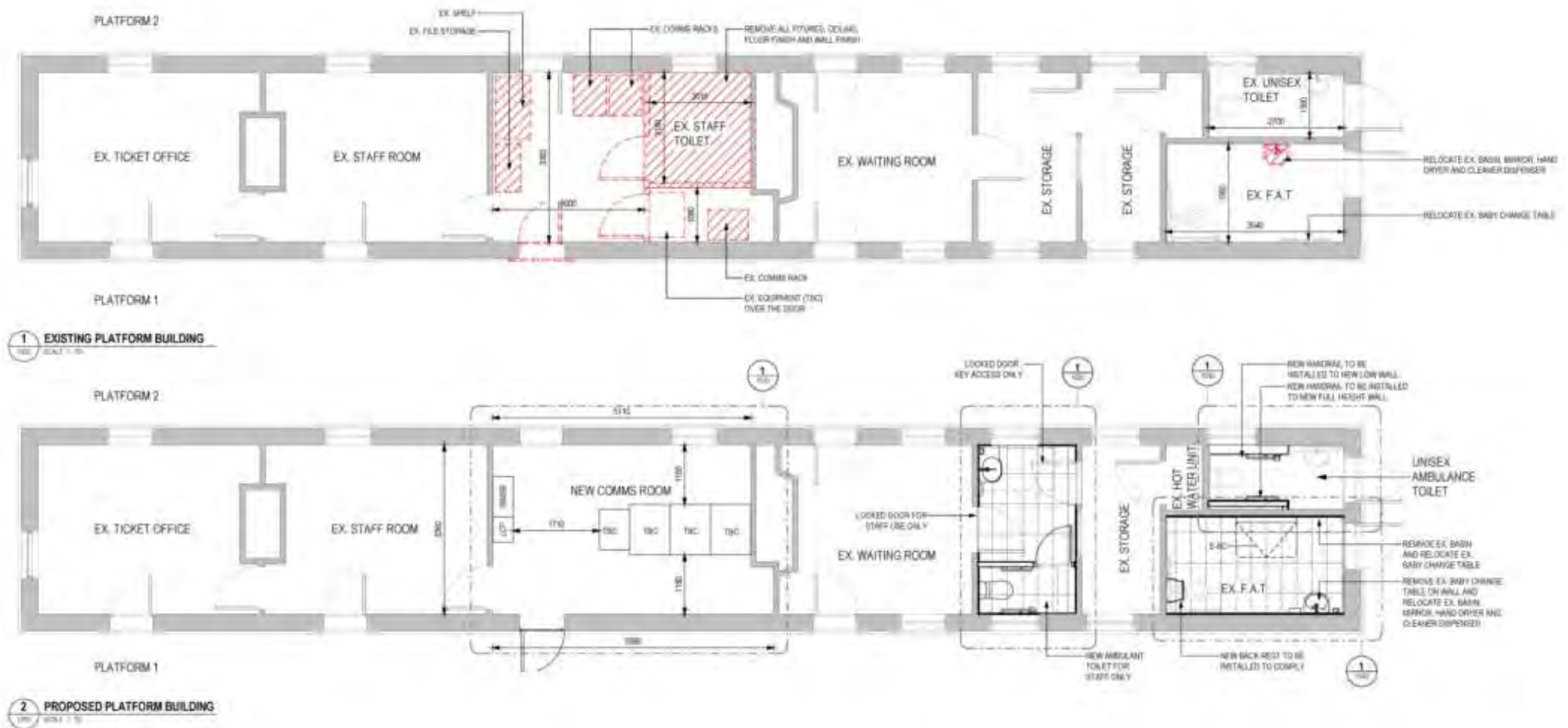
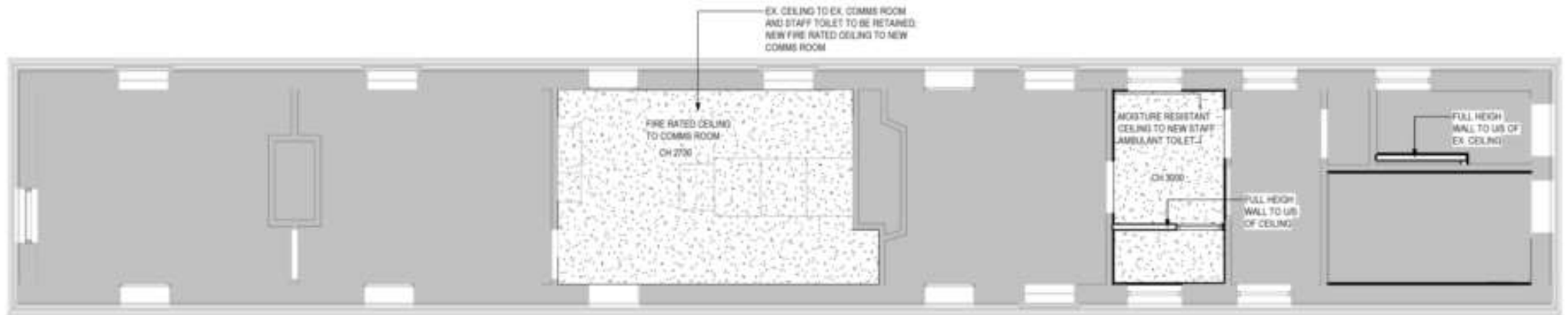


Figure 27. Detail of Works proposed to the interior of the Island Platform building. (Source: Transport for NSW)



3 PLATFORM BUILDING RCP
SCALE 1:50

Figure 28. Detail of Works proposed to the interior ceilings of the Island Platform building. (Source: Transport for NSW)

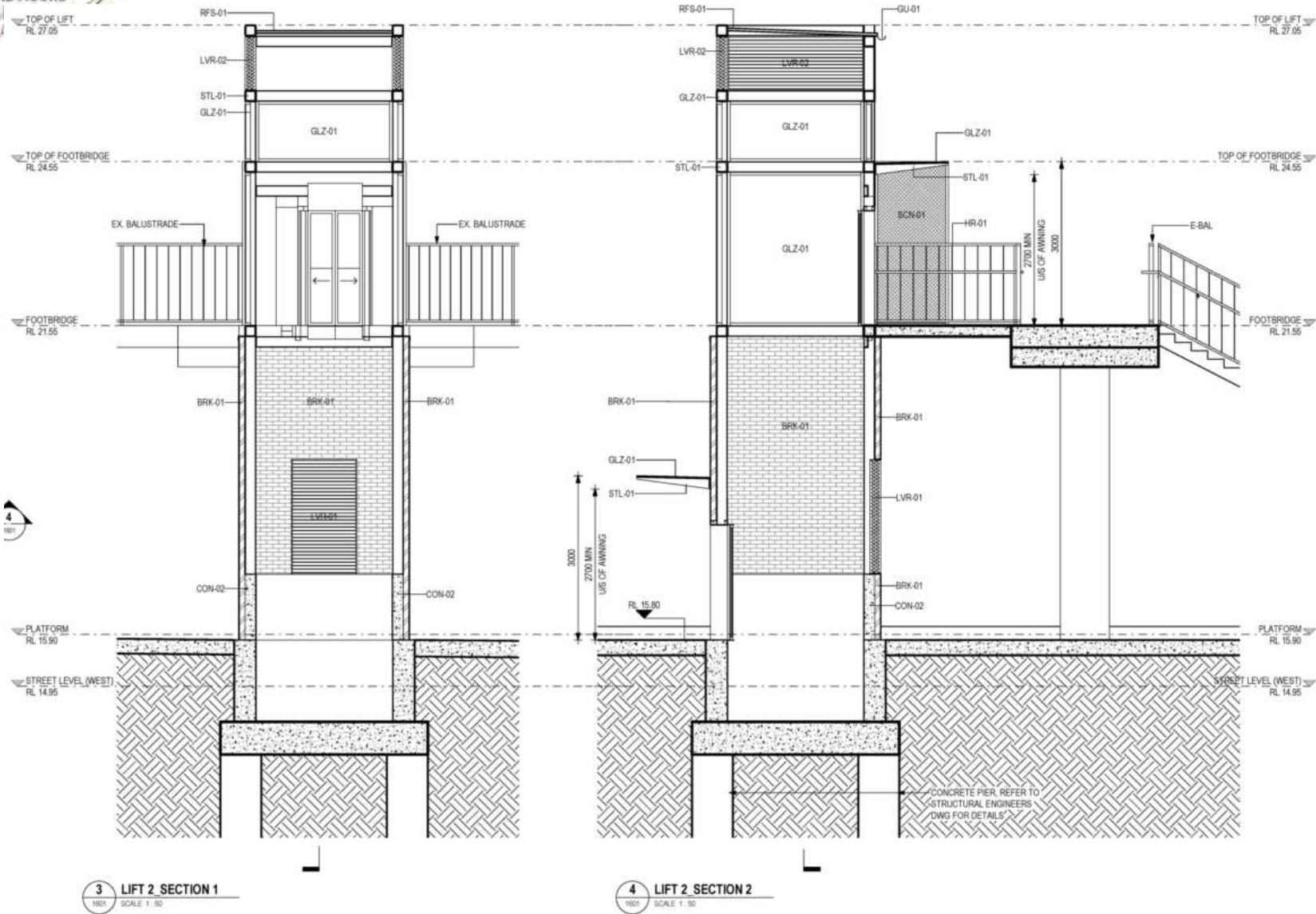


Figure 29. Sections of the Proposed Lift 2 to the Island platform (Lift 3 similar) (Source: Transport for NSW)



Figure 30. Design Projection (view to north-west) of the arrangement of the stairs and lift at the Queen Street end of the overhead bridge. (Source: Transport for NSW)

8. Impact Assessment

8.1 Discussion of Impacts

The following table identifies the individual components of the proposed works and considers the potential impacts of the works upon the heritage significance of North Strathfield Railway Station.

Proposed Work - Component		Assessment of Impact
Eastern Entrance - Queen Street		
Lift Access	<ul style="list-style-type: none"> ◆ New lift L1 including entry landings at street & footbridge level and new canopy to lift entry at footbridge level. 	<p>There is no heritage fabric associated with the Queen St pedestrian entry, nor the overhead pedestrian bridge. There are no significant views or vistas affected by these changes.</p> <p>The present design of the Proposal in this vicinity has minimised any effects upon the Queen Street landscaping and, whilst there would be some temporary effects, (for example: disturbance of ground, clearing of garden beds, creation of vehicle paths) to the gardens during construction, the long-term effects are considered to be negligible.</p> <p>This component of the Proposal would have no heritage impact on the Queen Street pedestrian entry, or on the station building and platforms as neither structures are visible from Queen Street owing to the dense foliage along the street. However, the lift will create an additional visual element in the landscape of the station but as simple, functional element which will deliver a new utility to the station its impact on the heritage significance of the overall station is</p>

Proposed Work - Component		Assessment of Impact
		considered as acceptable.
Pedestrian road safety improvements	<ul style="list-style-type: none"> ◆ New TGS1 to existing pedestrian crossing ◆ New minimum 1.8 m wide concrete foot path to comply with AS1428 ◆ Provision of one new DDA-compliant on-street parking space close to the station entrance on Queen Street. ◆ New kerb and kerb ramp for dedicated accessible parking and accessible kiss & ride ◆ New re-directed footpath - protect existing heritage trees 	<p>The only heritage fabric associated with this location is the trees planted between the footpath and the rail corridor. These works would not affect these trees. There are no significant views or vistas affected by these changes.</p> <p>This component of the Proposal would have no heritage impact.</p>
Western Entrance – Hamilton Street		
Lift Access	<ul style="list-style-type: none"> ◆ New lift L3 including entry landing at footbridge level and new canopy over lift entry at footbridge level remove section of footbridge balustrade. Handrail and kerb as required ◆ New compliant walkway and pavement from new lift L3 to existing stair on Platform 3 to comply with AS1428 cross falls & gradient, protect, retain - make good existing canopy. 	<p>There is no heritage fabric associated with the footpath and kerb from Hamilton Street to the western station entrance. There are no significant views or vistas impacted by these changes.</p> <p>The works necessary for the installation of the lift would affect only recent pavements and subsurface fill materials of no significance.</p> <p>The canopy is attached to the new fabric of the lift and would not affect any heritage fabric.</p> <p>The stairs are part of the relatively recent overhead bridge. The fencing of the ‘under-stairs’ area would not have any</p>

Proposed Work - Component		Assessment of Impact
	<ul style="list-style-type: none"> ◆ New canopy to lift L3 ground level entry ◆ Under stairs area to be fenced off 	<p>heritage impact.</p> <p>The lift will create an additional visual element in the landscape of the station but as simple, functional element which will deliver a new utility to the station its impact in terms of the overall heritage significance of the station building and platforms is considered as acceptable.</p>
Pedestrian road safety improvements	<ul style="list-style-type: none"> ◆ Modified footpath and kerb ramp for DDA parking at the Hamilton Street entrance. ◆ Added TGSi on the existing footpath at the Hamilton Street entrance. ◆ New minimum width 1.8m concrete footpath to comply with AS 1428 ◆ Modify Handrails as required ◆ New Bench Seats At 60 Metre Centres (4 Nos) 	<p>There is no heritage fabric associated with the footpath and kerb at Hamilton Street, nor with the footpath from Hamilton Street to the western station entrance. There are no significant views or vistas affected by these changes.</p> <p>This component of the Proposal would have no heritage impact on the station building or platforms as neither of these structures are visible from Hamilton Street.</p>
Island Platform (Platforms 1 & 2)		
Lift Access	<ul style="list-style-type: none"> ◆ New lift L2 including entry landing at footbridge level and new canopy over lift entry at footbridge level - remove existing section of footbridge balustrade, handrail & kerb as required. ◆ New canopy to lift L2 	<p>The only heritage fabric associated with this Lift location is the platform surface. The works necessary for the installation of the lift would affect only recent pavements and subsurface fill materials of no significance.</p> <p>The canopy would be attached to the new fabric of the lift and would not affect any heritage fabric.</p>

Proposed Work - Component		Assessment of Impact
	<p>ground level entry</p> <ul style="list-style-type: none"> Under stairs area to be fenced off 	<p>The stairs are part of the relatively recent overhead bridge. The fencing of the 'under-stairs' area would not have any heritage impact.</p> <p>Whilst this new lift would sit within an important view or vista along the railway line to the south, this view is already dominated by the new concrete overbridge and these additional structures would be consistent with this element and would form a component of it. This may have a minor adverse effect upon the nature and extent of this view but it would have no substantive impact on the heritage values of North Strathfield Station. There has always been a pedestrian bridge in this location.</p> <p>The lift will create an additional visual element in the landscape of the station but as simple, functional element which will deliver a new utility to the station its impact in terms of the overall heritage significance of the station building and platforms is considered as acceptable.</p>
<p>Pedestrian safety improvements</p>	<ul style="list-style-type: none"> Platform pavements re-graded as required to provide a compliant accessible pathway from new lift to station amenities Handrails required to family accessible toilet entry area to ensure DDA compliance 	<p>The only heritage fabric associated with this element of the Proposal is the platform surface. The works necessary for the reconfiguration of the platform surfaces would affect only recent pavements and subsurface fill materials of no significance.</p> <p>There is no heritage fabric directly associated with the location of the handrails for the family accessible toilet entry. However, they would be a new element in close proximity to the Platform building. This would have a minor adverse effect upon the view of the building from the north but, as such elements are common and utilitarian in a positive</p>

Proposed Work - Component		Assessment of Impact
		<p>context, their installation would have no substantive impact on the heritage values of North Strathfield Station.</p> <p>This component of the Proposal would have no heritage impact.</p>
Platform Building Alterations	<ul style="list-style-type: none"> ◆ Conversion of the Existing Staff Toilet and 'Comms' Room to a single large 'Comms' room. ◆ Conversion of the existing northern store room into a staff toilet ◆ Refitting of the interiors of the existing Unisex Ambulant and Family Accessible toilets 	<p>This work would involve the removal of existing non-original interior fittings and fixtures to reinstate the original single room space. This would then be fitted with free-standing 'Comms' equipment. The only alteration to existing fabric is to the existing pedestrian door to the western platform which will be re-hung to swing outwards. The majority of this work represents the removal of existing intrusive fabric and is a positive heritage impact. The re-hanging of the door is a minor adverse impact which is reversible and of little long-term consequence, as the original arrangement will be obvious in the door framing and can be returned in the future.</p> <p>The creation of a staff toilet in this space represents a new interior fitout of this room with little impact upon the original fabric of the building. Whilst it represents a change of use, it would have no substantive heritage impact, and can therefore be characterised as a minor change.</p> <p>The present interior fittings and finishes of these two toilet spaces are non-original and of relatively recent origin. Their removal and replacement with new fittings and new finishes would not affect any heritage fabric. Whilst this work represents a change of interior finishes, it would have no substantive heritage impact.</p>

8.2 Summary of Heritage Impacts

Built Heritage

Overall, majority of the components of the Proposal would not have any impact upon significant fabric of North Strathfield Station, as they are related to the roadside and pedestrian access pathways leading to the Station. There is no direct or indirect effects upon the significant trees located along Queen Street.

The installation of the pedestrian lifts from the overhead bridge to the platform level would add a set of new structures to the station but would not directly affect any heritage fabric to any substantive degree. The present overhead bridge is relatively new and formed of reinforced concrete and these new structures would be materially and visually consistent with this material and the visual context created by the bridge. The lift towers would be notable additional visual elements in the landscape of the station but they are simple, functional elements which are common in similar environments and unremarkable. Their functionality and their essential character as the expression of a necessary and welcome new utility provide a context which justifies some degree of impact as reasonable and, consequently, acceptable.

The lift's location in relation to the significant building on Platform 1 / 2 is sufficiently detached that they would not affect any views to the Station and only marginally affect views away from the station. The excavations and minor alterations to the platform pavements to allow the footings for the bases of the lifts would not have any heritage impact.

The regrading of pavements and adjustments to the platform surfaces to make them more accessible are minor works which would not have any adverse impact upon heritage fabric, nor would the addition of TGSI upon the pavements. Platform pavements are utilitarian fabric of little heritage significance and their rearrangement to better serve their purpose is consistent with the operation of the railway station as a public transport facility.

The installation of handrails for the family accessible toilet entry would be in close proximity to the Platform building but, as such elements are common and utilitarian in a positive context, their installation would have no substantive impact on the heritage values of North Strathfield Station.

Similarly, new and additional wayfinding signage, which would be consistent with the Sydney Trains Signage Policy (for Heritage Items), is a minor change of negligible heritage impact.

Impact Assessment Summary Station Building (1918)

Grading of Significance	HIGH
Proposed work	Larger comms' room. Conversion of northern store room into staff toilet Refitting interiors of Unisex Ambulant and Family Accessible toilets
Rationale	Upgrading station facilities
Degree of Impact	Minor impact with very little original fabric affected by proposed works.

Impact Assessment Summary Platforms (c1918)

Grading of Significance	HIGH
Proposed work	Installation of the two lifts to platforms – L2 to island platform 1 and 2, and L3 to side platform 3. This would involve demolition of a small section of concrete slab of the platforms; compliant walkway and pavement from new lift L3 to existing stair on platform 3; regarding sections of all three platforms
Rationale	Demolition of concrete slab required to accommodate lift shaft; regarding required as part of station upgrade.
Degree of Impact	Moderate impact to small section of concrete slab affected by lift installations as impacts are to 1918 platform fabric; Minor impact to platform surfaces as these have been resurfaced before

Impact Assessment Summary Footbridge (1992)

Grading of Significance	LITTLE
Proposed work	Entry landings for 3 lifts at footbridge level
Rationale	Lift landings needed to connect footbridge to platform level, and street level
Degree of Impact	Minor impact – the lift landing would be an extension to the footbridge and would change its setting noticeably;

Impact Assessment Summary Modern Steel Shelters (c1990)

Grading of Significance	LITTLE
Proposed work	None
Rationale	-
Degree of Impact	No change

Impact Assessment Summary Moveable

Grading of Significance	MODERATE
Proposed work	None
Rationale	-
Degree of Impact	No change

Station Landscaping

Impact Assessment Summary Landscape

Grading of Significance	MODERATE
Proposed work	New re-directed footpath along Queen Street at a distance from existing heritage trees
Rationale	To enable greater and more directed access to the station entrance
Degree of Impact	Negligible impact – the trees are at a distance from the proposed footpath

Views and settings

The Proposal would create three new vertical elements associated with the three lift structures. In all three cases, there are no significant views to or from the North Strathfield Station affected in a negative manner. The station itself sits in a cutting and there are limited views of the platform building from any public area apart from on the pedestrian overbridge. From most external aspects, these modern structures would be consistent with the general view-scape and context, leaving the platform building as a singular and picturesque element within the railway cutting. The proposed works would not have an adverse impact upon the views of the platform building.

North Strathfield Station has already been notably changed by the construction of the NSRU Line and the replacement of the pedestrian overbridge with the modern concrete structure. These elements surround the island platform and frame the central platform building. In the light of the general suburban context of large modern buildings and the mixed railway context, these new structures would not have a substantive adverse impact upon the setting of the island platform and its station building.

Curtilage and subdivision

The Proposal would have no impact on North Strathfield Railway Station's curtilage or subdivision.

Heritage in the vicinity

There would be no impacts to heritage items in the vicinity.

9. Statutory Context and Approvals

9.1 Introduction

A number of planning and legislative documents govern how heritage is managed in New South Wales and Australia. The following section provides an overview of the requirements under each as they apply to the Proposal.

9.2 Commonwealth Legislation

Environment Protection and Biodiversity Conservation Act 1999

The Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act) defines 'environment' as both natural and cultural environments and therefore includes Indigenous and non-Indigenous historic cultural heritage items. Under the Act protected heritage items are listed on the National Heritage List (items of significance to the nation) or the Commonwealth Heritage List (items belonging to the Commonwealth or its agencies).

Under Part 9 of the EPBC Act, any action that is likely to have a significant impact on a matter of national environmental significance (known as a controlled action under the Act), may only progress with approval of the relevant minister. An action is defined as a project, development, undertaking, activity (or series of activities) or alteration. An action also requires approval if:

- It is undertaken on Commonwealth land and will have or is likely to have a significant impact on the environment on Commonwealth land; and/or
- It is undertaken by the Commonwealth and will have or is likely to have a significant impact.

A search of relevant heritage lists indicates that there are no places of National or Commonwealth Heritage significance in or around the station curtilage, and therefore the Proposal would not require referral under the EPBC Act with respect to heritage.

9.3 State Legislation

Environmental Planning and Assessment Act 1979

For environmental assessment purposes under Part 5 of the NSW *Environmental Planning and Assessment Act 1979*, Subdivision 2, Section 5.5 of the Act requires that a determining authority: "take into consideration such of the following matters as are of relevance to the development" with respect to the proposed works. Heritage matters fall within the scope of 'environment' with respect to this Act.

Conclusion

This report fulfils the assessment requirements under Part 5 of the EP&A Act.

NSW Heritage Act 1977

North Strathfield Railway Station is not listed on the State Heritage Register, however, it is included in the RailCorp Heritage and Conservation Register, maintained under Section 170 of the Heritage Act.

Section 170A of the Act addresses the management of heritage items by Government instrumentalities:

Heritage management by government instrumentalities

- (1) *A government instrumentality must give the Heritage Council not less than 14 days written notice before the government instrumentality:*
 - (a) *removes any item from its register under section 170, or*
 - (b) *transfers ownership of any item entered in its register, or*
 - (c) *ceases to occupy or demolishes any place, building or work entered in its register.*
- (2) *Each government instrumentality is responsible for ensuring that the items entered on its register under section 170 and items and land to which a listing on the State Heritage Register applies that are under its care, control or management are maintained with due diligence in accordance with State Owned Heritage Management Principles approved by the Minister on the advice of the Heritage Council and notified by the Minister to government instrumentalities from time to time.*

State-Owned Heritage Management Principles

The State-Owned Heritage Management Principles are contained in the *State Agency Heritage Guide*, which contain the seventeen heritage ‘Principles’ and the ‘Heritage Asset Management Guidelines’.

Conclusion

The State-owned Heritage Management Principles require that decisions relating to the management of the railway station should adopt “*appropriate heritage management strategies, processes and practices*” and that the “*public sector should set the standard for the community in the management of heritage assets*”.

The key relevant ‘Principles’ are:

4. Conservation Outcomes

Heritage assets should be conserved to retain their heritage significance to the greatest extent feasible. State agencies should aim to conserve assets for

operational purposes or to adaptively re-use assets in preference to alteration or demolition

7. Appropriate Uses

Heritage assets should, where feasible, continue to be maintained in their operational role. Where they are surplus to operational requirements, State agencies should aim to ensure that items are adaptively re-used for a purpose sympathetic to their heritage significance.

The preparation of this report and its conclusions represent partial fulfilment of this requirement.

State Environmental Planning Policy (Infrastructure) 2007, (INFRASTRUCTURE SEPP)

The *Infrastructure SEPP* allows for some impacts to State and Local heritage items without development consent, subject to an assessment of the heritage impacts. Clause 14 of the *Infrastructure SEPP* requires consultation with the relevant local council for works impacting local heritage items on environmental planning instruments or being undertaken within a heritage conservation area, unless those works are “*minor or inconsequential*”³. If works are not “*minor or inconsequential*”, a written assessment must be undertaken of the impacts of the proposed works upon the heritage item or conservation area⁴. Clause 20(2)(e) of the *Infrastructure SEPP* requires that, for works to be exempt, they must have no more than a “*minimal impact*” on items of State or local heritage significance.

The proposed works are considered to be in keeping with the definition of “rail infrastructure facilities” as per section 78 of the *Infrastructure SEPP*. Specifically, item (d) *railway stations, station platforms and areas in a station complex that commuters use to get access to the platforms*.

Conclusion

The proposed work is considered to have a “*minor and inconsequential*” impact on the heritage significance of North Strathfield Railway Station. In accordance with Clause 20(2)(e) of the *Infrastructure SEPP*, this work is exempt. Additionally, because of the minor nature of the proposal’s impact, no consultation or notification is required with Canada Bay Council in relation to this specific proposal, in accordance with Clause 14 of the *Infrastructure SEPP*.

Canada Bay Local Environmental Plan 2013

Clause 5.10 of the Canada Bay LEP 2013 contains provisions for the protection of heritage items and conservation areas within the municipal area. Clause 5.10(4) requires that

³ Ibid. Section 14(1)(a)

⁴ Ibid. Section 14(2)(a) - (c)

Council must assess the heritage significance of the item or conservation area and take into account the extent of the impact of the proposed works on the heritage significance of the site or area. However, under the provisions of the Infrastructure SEPP the works are exempt from the requirement to seek development consent from Canada Bay Council.

The only heritage item identified in the Canada Bay LEP 2013 which is in the vicinity of the proposed works is the item: *Street Trees - Adj. To North Strathfield Railway Station*. These trees would not be affected by the proposed works.

Conclusion

As the works are fully contained within North Strathfield Railway Station and relate specifically to approved rail-infrastructure activities, the provisions of the *Infrastructure SEPP* apply regarding “Development without Consent”. Additionally, the works are assessed as having no impact on heritage significance, therefore consultation with Canada Bay Council is not required.

10. Conclusions and Recommendations

10.1 Conclusion

This Statement of Heritage Impact has considered the history and significance of North Strathfield Railway Station, so as to assess the heritage impact of the proposed North Strathfield Station Upgrade. The Proposal aims to make North Strathfield Railway Station physically accessible to the widest range of people, including those with limited mobility.

Generally, the Proposal would improve the utility of the station by improving its accessibility, without having a substantive adverse impact on heritage fabric. The proposal has been developed using the conservation principle of “as much as necessary, as little as possible”, opting for retention of heritage fabric as much as possible and respect for the overall aesthetic of the station. The project makes a conscious effort to retain and respect the heritage significance of the station, whilst installing necessary new infrastructure.

The Proposal confines the majority of the works to the modern overhead pedestrian bridge located at the southern end of the platforms and would not directly affect any other railway buildings or structures. Other works relate to footpaths and roadways leading to the station, none of which would affect any significant heritage fabric. There are minor changes to the platforms and their pavements but these impacts are minor and acceptable.

Other works around the platform building do not affect any significant heritage fabric and do not create any significant adverse impacts upon the setting of, or views to and from, the Station. There would be no impact to curtilage, subdivision, Heritage Conservation Areas or Heritage Items in the vicinity.

10.2 Recommendations

Based upon the analysis and conclusions carried out above, the following recommendations should be considered:

- A heritage induction would be provided to workers prior to construction, informing them of the location of known heritage items and guidelines to follow if unanticipated heritage items or deposits are located during construction.
- In the event that any unanticipated archaeological deposits are identified within the project site during construction, the procedures contained in TfNSW’s *Unexpected Heritage Finds Guideline* would be followed and works within the vicinity of the find would cease immediately. The Contractor would immediately notify the TfNSW Project Manager and the TfNSW Environment and Planning Manager so they can assist in co-ordinating the next steps which are likely to involve consultation with an archaeologist and OEH. Where required, further archaeological work and/or consents would be obtained for any unanticipated archaeological deposits prior to works recommencing at the location.

- A heritage architect should be engaged for the detailed design process and to inform the detailed design recommendations. Specifically:
 - The heritage architect should advise on the materials and finishes palette.
 - The heritage architect should advise on the design of the new upper addition of the former Booking Office. Any new materials should aim to reproduce the original as closely as possible.
 - The heritage architect should advise on the design of the balustrade around the platform stairs. This is with respect to the relocation of panels and the design of mesh infill additions between the rails.
- Prior to the commencement of works, contractors must be briefed on the heritage sensitive nature of the site and informed of any recommended mitigation measures or controls required.
- Any accidental damage to a heritage item is to be treated as an incident, with appropriate recording and notification.
- During construction, suitable measures would be put in place to ensure the retained heritage elements are protected from damage. Measures may include hoardings, use of spotters during the movement of equipment and other measures as necessary.
- On completion of works, an update would be prepared for the Section 170 Heritage and Conservation Register, with required details.

Provided the above mitigation measures are put in place, the heritage impact of the Proposal is considered to be acceptable.